

**PR8 Land East of the A44  
Development Brief Draft  
Cherwell District Council  
August 2023**



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# PR8 Land East of the A44 Development Briefs *Draft* Cherwell District Council August 2023

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Grade II listed Begbroke Hill Farmhouse

# Executive Summary

## Vision and development framework

The site specific vision for Land East of the A44 is as follows and is explored in Chapter 5 of the Development Brief:

The Cherwell Local Plan 2011-2031 (Part 1) Partial Review (LPPR), which provides for Cherwell's share of Oxford City's unmet housing needs, identifies Land East of the A44 at Begbroke as one of six strategic housing sites. A comprehensive Development Brief is required as guidance for future planning applications.

This Development Brief has been jointly prepared between Cherwell District Council, Oxfordshire County Council, landowners and key stakeholders. It is a material planning consideration in the determination of any future planning applications for the site.

The Development Brief includes a review of the site's context including the LPPR strategic vision and spatial strategy and the site specific development constraints and opportunities. Based on this analysis it goes on to provide a site specific vision and comprehensive development principles addressing land use, character, layout, green infrastructure, movement, utilities, healthy place making and sustainable design.

## Site location

The site comprises 190 hectares of land to the east of the A44 and between the residential neighbourhoods of Begbroke to the north west and Yarnton to the south west. Begbroke Science Park and Yarnton Home and Garden are located in the northern part of the site, with the rest of the site generally in agricultural use. To the east, the site is bounded by the Oxford Canal, beyond which lies the village of Kidlington. To the north/north-east, the site is bounded by farmland and the Rushy Meadow SSSI, beyond which is the Langford Locks Industrial Estate and London Oxford Airport. To the south, the site is bounded by Littlemarsh Playing Field and a disused Sewage Treatment Works. Rowel Brook crosses the northern part of the site, a rail line runs north-south through the eastern part of the site.

*The expansion of the University's Begbroke Science Park will be integrated within a distinctive, urban neighbourhood featuring innovation and high levels of sustainability, which will become recognised as a national placemaking exemplar. The new neighbourhood will have walking and cycling at its core, by creating an environment where the private car is a guest whilst catering for public transport. It will be publicly accessible, well connected by footpaths and cycleways to the surrounding neighbourhoods, existing local services and enhanced public transport links on the A44.*

*The development will be a distinct and unique blend of green space, water environments and natural spaces promoting good health habits. A new local centre, schools and green infrastructure corridors adjacent to the Oxford Canal will link existing communities with publicly accessible open spaces of high quality wildlife and biodiversity.*

*It will be successfully connected to Begbroke, Yarnton and Kidlington which will have retained their own identities.*

Policy PR8 of the LPPR sets out the policy requirements for the site which include the following key elements:

- 1,950 homes on approximately 66 hectares of land seeking 50% affordable housing provision across the site to meet Oxford's housing needs
- secondary school on 8.2 hectares of land to incorporate a 4-court sports hall to Sports England Specification and available for community use
- primary school with three forms of entry on 3.2 hectares of land
- primary school with two forms of entry on 2.2 hectares of land
- local centre on one hectare of land

- reservation of 14.7 hectares of land for Begbroke Science Park expansion
- Local Nature Reserve on 29.2 hectares of land
- nature conservation area on 12.2 hectares of land
- public open space on 23.4 hectares of land
- retention of 12 hectares of agricultural land
- reservation of 0.5 hectares of land for a future railway halt
- provision for a foot, cycle and wheel chair accessible bridge over the Oxford Canal linking the site with land at Stratfield Farm (Policy PR7b)
- facilities for formal sports, play areas and allotments.

The Development Framework plan (overleaf) reflects the vision and the requirements of Policy PR8. In preparing the brief, detailed evidence has been considered leading to variations to the location of specific uses as allowed by policy. The justification for these changes is explained further in the brief.

Detailed design requirements which underpin the delivery of this development framework are set out in the Chapter 6 of the Development Brief. Chapter 7 lists the information which will be required to accompany a planning application.



View east across the site towards the railway line

Executive Summary

- Site Boundary
- Site Boundary of adjoining sites
- Primary land use
  - Residential
  - Employment
  - Employment or residential
  - Primary school\*
  - Secondary school\*
  - Mixed use – retail, commercial, residential
    - 1. Retained Yarrton Home & Garden
    - 2. Local centre
    - 3. Convenience facilities
  - New public green space/parks/wildlife areas
  - Proposed green spaces within developable area\*\*
  - Local Nature Reserve
  - Nature Conservation Area (limited public access)
  - Revised Green Belt
  - Retained Agricultural Land
  - SSSI
  - NIERC Act. 541
  - Priority Habitat Inventory
  - Broad-leaved semi-natural woodland
  - Retained hedgerows
  - Reinstated hedgerow
  - Tree considered to be of veteran status
  - Tree considered to be of transitional veteran status
  - Other retained trees
  - Retained group of trees
  - Soft edge
  - Ditch retained and integrated into site drainage
  - Equipped play area (indicative location)
  - Parkland/visitor hub/play space
  - Noise attenuation (size/type to be confirmed)
  - Vehicular access (southern location is indicative)
  - Proposed A44 pedestrian/cycle crossing
  - National Cycle Route 5
  - Existing/improved PROW/cycle routes (indicative)
  - Key new walking/cycling routes
  - Zone for potential rail halt
  - New pedestrian/cycle bridge over railway/canal
  - Potential for alternative bridge further north with restricted vehicular access, subject to agreement between OUD and Network Rail

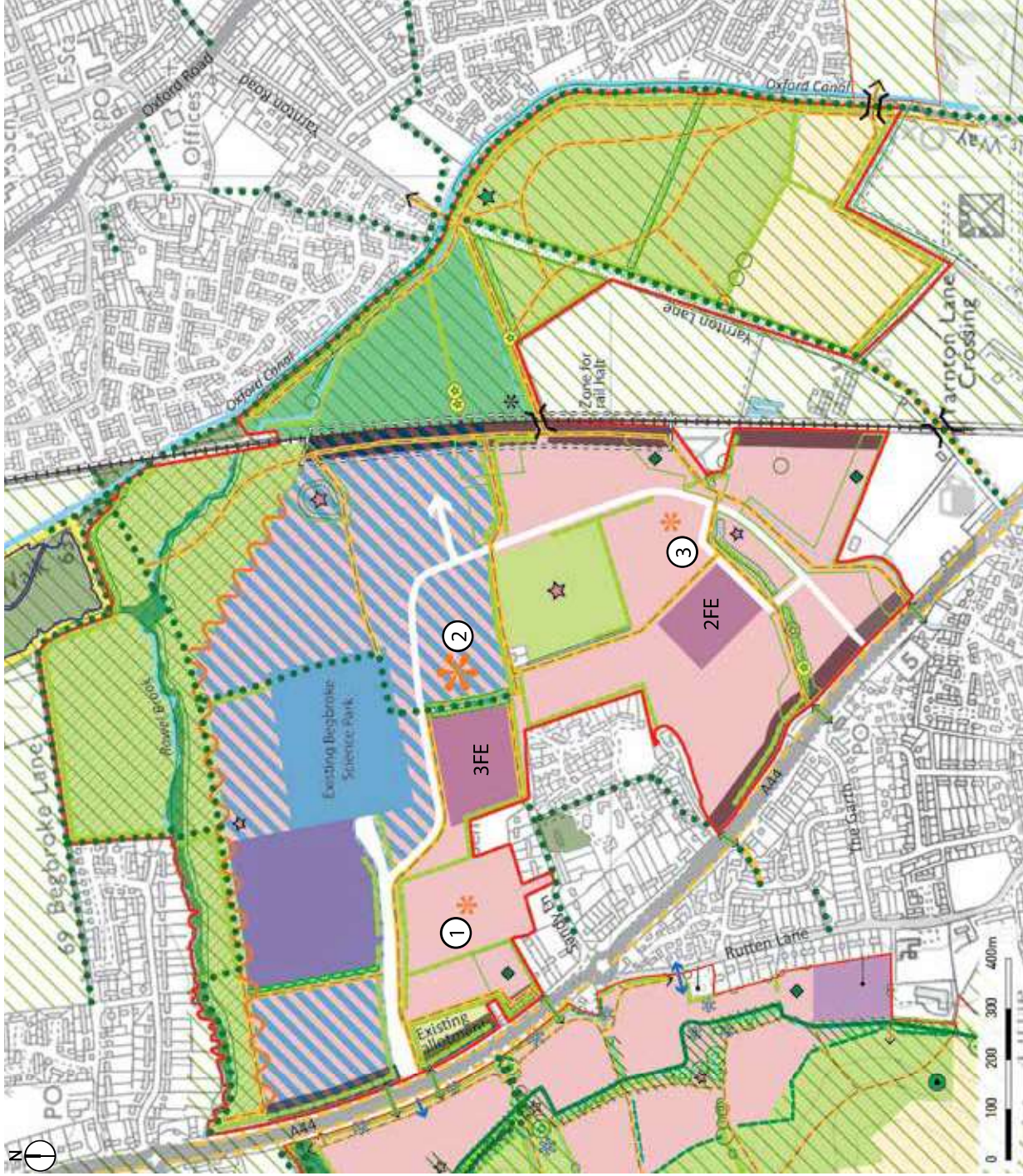


Figure 1: Development framework

\*School site location subject to further detailed assessment  
 \*\*Built-up area to incorporate amenity space as per adopted standards

# 1.0 Introduction

## 1.1 Background

The Cherwell Local Plan 2011-2031 (Part 1)<sup>1</sup> which was adopted in July 2015 (“The 2015 Plan”) committed the Council to work jointly with other Oxfordshire councils to assess the extent of the housing need that could not be met elsewhere in the Oxfordshire Housing Market Area. In particular, it was understood that there could be a need arising from Oxford that could not be met by Oxford City Council due to its tight administrative boundaries and its limited supply of land. Cherwell District Council’s commitment was to consider the extent of the need and, if necessary to ‘partially review’ its Local Plan.

The Council has now undertaken this ‘partial review’ with the adoption of the Cherwell Local Plan 2011-2031 (Part 1) Partial Review – Oxford’s Unmet Housing Need in September 2020 (LPPR)<sup>2</sup>.

The Partial Review which is effectively a supplement or addendum to the 2015 Plan, provides a vision, objectives and specific policies for delivering additional development to help meet Oxford’s housing needs. It seeks to do this in a way that will best serve Oxford’s needs and provide benefits for existing communities in Cherwell and adjoining areas.

The LPPR provides for the development of a total of 6 strategic housing sites that will best achieve the Council’s vision and objectives and deliver sustainable development of, in total, 4,400 new homes to meet Oxford’s needs together with supporting infrastructure. The LPPR requires single comprehensive, outline schemes for the entirety of each strategic site.

Each of the site allocations has a policy which sets out its key delivery requirements and place shaping principles, and each allocation is supported by a Policies Map.

<sup>1</sup> [Local Plan Part 1](#) <sup>2</sup> [Local Plan Partial Review Sep 2020](#)

Each of the site allocation policies requires planning application(s) for the site to be in accordance with a Development Brief for the site which has been jointly prepared by the landowner(s), Cherwell District Council, Oxfordshire County Council and other stakeholders, including Oxford City as appropriate. The site allocation policy also sets out a series of requirements that the Development Brief should address.

This is the development brief to guide the development of Land East of the A44, site PR8. The Development Brief has been prepared in accordance with policy requirements, the site allocation policy and the Policies Map. As well as including the required detail, the Development Brief also reflects the detailed key delivery requirements and place shaping principles as set out in the policy.

Site	Housing allocation	
<b>North Oxford</b>		
Policy PR6a – Land East of Oxford Road	690	N.B. Site allocation PR6c shown on Figure 2 is the allocation of Land at Frieze Farm which is reserved for the potential construction of a golf course should this be required as a result of the development of Land West of Oxford Road under Policy PR6b.
Policy PR6b – Land West of Oxford Road	670	
<b>Kidlington</b>		
Policy PR7a – Land South East of Kidlington	430	
Policy PR7b – Land at Stratfield Farm	120	
<b>Begbroke</b>		
Policy PR8 – Land East of the A44	1950	
<b>Yarnton</b>		
Policy PR9 – Land West of Yarnton	540	
<b>Total</b>	<b>4400</b>	

Housing allocations (LPPR)



- Allocation site boundaries
- Key sustainable movement routes (public transport and/or walking/ cycling)

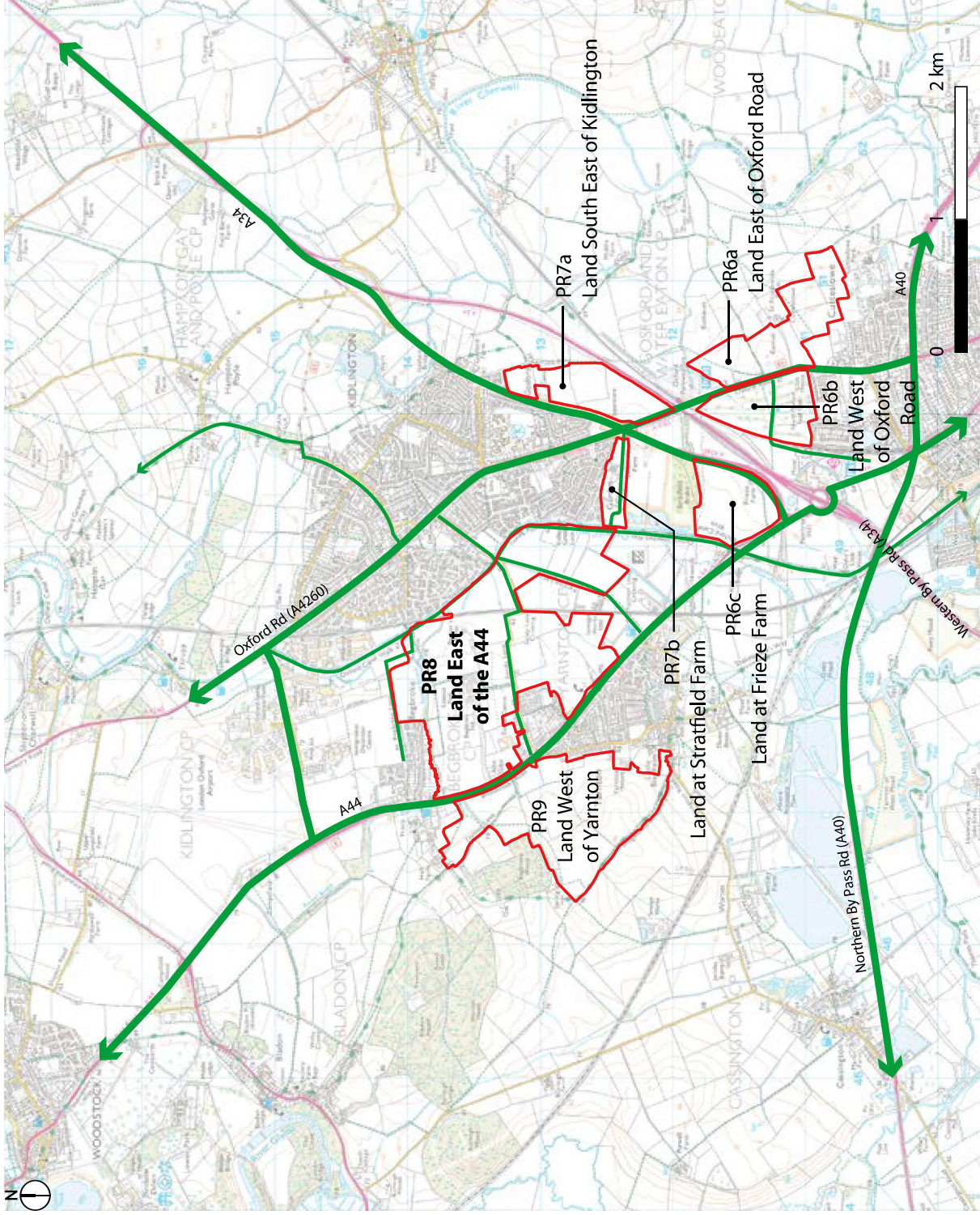


Figure 2: Local Plan Partial Review Site Allocations Location

## 1.2 Purpose and status of the Development Brief

### 1.2.1 Purpose

The Development Brief has 4 main objectives:

- To create a site specific vision to guide future site development in a manner which supports the wider aims of the LPPR spatial strategy for North Oxford, Kidlington and the A44/A4260 Corridors
- To provide a development framework and a clear set of site specific development principles to inform the submission and determination of planning applications and achieve comprehensive and holistic development in accordance with the LPPR site policy
- To improve the efficiency of the planning and development process by reducing uncertainty and setting a framework for development that provides landowners, developers and the wider community with clear guidance on what is expected from development
- To raise the standard of design and to create exemplary places which are functional, beautiful and which engender a sense of community.

The Development Brief, where necessary and appropriate, proposes or reflects solutions and proposals outside the individual site boundary to help facilitate a joined up approach to development.

The Development Brief should be read in conjunction with relevant Development Plan policies, national planning policy and guidance and the Council's adopted Supplementary Planning Documents ("SPD"). Particular attention is drawn to the Council's design policies and guidance including Policy ESD15: The Character of the Built and Historic Environment, and the Cherwell Design Guide SPD which provides design guidance relevant to the District as a whole. Further information on relevant Policy and guidance is provided in chapter 3 and throughout the Development Brief.

### 1.2.2 Status

The Development Brief has been endorsed by Cherwell District Council's Planning Committee. It will be used as a material planning consideration in the determination of any planning applications for the site.

For the avoidance of doubt, the Development Brief does not have the status of a Supplementary Planning Document nor does it introduce new planning policy. The brief is a policy requirement and planning applications will be expected to be prepared in accordance with the brief.

Landowners have been engaged in proactive and positive discussions with the Council and other stakeholders as the Brief has been prepared. This work is ongoing. Should final proposals differ from elements of the Development Brief in due course, in response to further technical and design work, some of which is not yet available to the Council, this would be examined through the planning application process in the usual way.

## 1.3 Structure of the Development Brief

The structure of the Brief is as follows:

**Chapters 1 to 3** provide contextual information relating to the site and the Development Brief process, including the strategic vision and spatial strategy for the North Oxford, Kidlington and A44 corridor.

**Chapter 4** provides a synthesis of policy context and important site constraints and opportunities which are to be reflected in the site's development. This builds on the LPPR Evidence Base.

**Chapter 5** describes the site specific vision and development objectives.

**Chapter 6** contains a comprehensive set of design and development principles for the site which respond to the site opportunities, constraints and context set out in the preceding chapters and which are to be reflected in planning applications.

**Chapter 7** lists the information which will be required to accompany a planning application.

## 1.4 Consultation and stakeholder engagement

The Development Brief has been jointly prepared by Cherwell District Council and the site owners and their representatives and in consultation with Oxfordshire County Council, Oxford City Council and the Canal and River Trust.

Throughout the process there has been engagement and consultation with the following stakeholders in addition to those mentioned above:

- Parish Councils
- Thames Valley Police
- Berkshire, Buckinghamshire and Oxfordshire Wildlife Trust (BBOWT)
- Thames Water
- Environment Agency
- Scottish and Southern Electric (SSE)
- Network Rail
- Natural England
- Sport England
- Integrated Care Board

This has included collaborative workshops focussing on key stages during the preparation of the Development Brief:

- Baseline review and analysis
- Vision and development principles

These collaborative workshops with specialist stakeholders, were preceded by a joint workshop in October 2018 with Parish Councils, landowners and their representatives and stakeholders. This workshop introduced the Development Brief process, provided an opportunity for site promoters and stakeholders to introduce themselves, and enabled Parish Councils to explain their aspirations/ requirements for the Development Briefs.

Technical information and emerging design work provided by the landowners and their representatives has been considered by the Council in preparing the Development Brief.

### 1.4.1 Community Engagement

Comments received during public consultation on the draft Development Brief will be used to inform the final Brief.

Details of the consultation are covered in a separate consultation notice.



Aerial view of site PR8

## 2.0 The Strategic Vision and Context

### 2.1 Local Plan Partial Review Vision

The LPPR vision across all sites is:

- To provide new development that meets Oxford's agreed, identified housing needs, supports the city's world-class economy, universities and its local employment base, and ensures that people have convenient, affordable and sustainable travel opportunities to the city's places of work, study and recreation, and to its services and facilities. This development will be provided so that it:
- i. creates balanced and sustainable communities
  - ii. is well connected to Oxford
  - iii. is of exemplar design which responds distinctively and sensitively to the local built, historic and environmental context
  - iv. is supported by necessary infrastructure
  - v. provides for a range of household types and incomes reflecting Oxford's diverse needs
  - vi. contributes to improving health and well-being, and
  - vii. seeks to conserve and enhance the natural environment.

*LPPR Vision for Meeting Oxford's Unmet Housing Needs in Cherwell*

To deliver this vision, the LPPR identifies sites for new homes in locations which have the strongest socio-economic connections to Oxford, and which can deliver the necessary social, movement and green infrastructure to support the health and wellbeing of the population.

The sites are located at the edge of existing communities to the north of Oxford and will be integrated with them to share the benefits of new facilities and support existing local centres, in particular Kidlington village centre.

#### 2.1.1 The Role of Individual Sites

Each site plays a role in delivering the vision and objectives of the LPPR, in a joined-up and holistic manner as shown on the LPPR key diagram Figure 3 overleaf, and thematic figures 4-7 which follow. Full details of each site's role are contained within LPPR policies.

#### **The role of land to the east of the A44 site (PR8)**

Land to the east of the A44 is located to the west of Yarnton, east of Kidlington and south east of Begbroke village. At its centre is Begbroke Science Park. The site is of sufficient scale to create a contemporary, high quality new urban neighbourhood served by its own local centre and schools.

The expansion of Begbroke Science Park will be integrated into the development to create a mixed use neighbourhood, which is connected to the village of Begbroke and provides a frontage to the A44. New streets and footpaths will enhance walking and cycling connections towards the higher order facilities at Kidlington village centre to support its long term success. Sandy Lane will provide



2.1.2 Economic relationships

The sites are located in close proximity to local centres, key employment sites and sites which have an important economic relationship with Oxford and form part of Oxfordshire’s ‘Knowledge Spine’. These include existing locations within Cherwell (Oxford Parkway Railway Station, London-Oxford Airport, Langford Lane commercial area in Kidlington and Begbroke Science Park) and within the city of Oxford (the Oxford North site), which will be a key driver of employment growth.

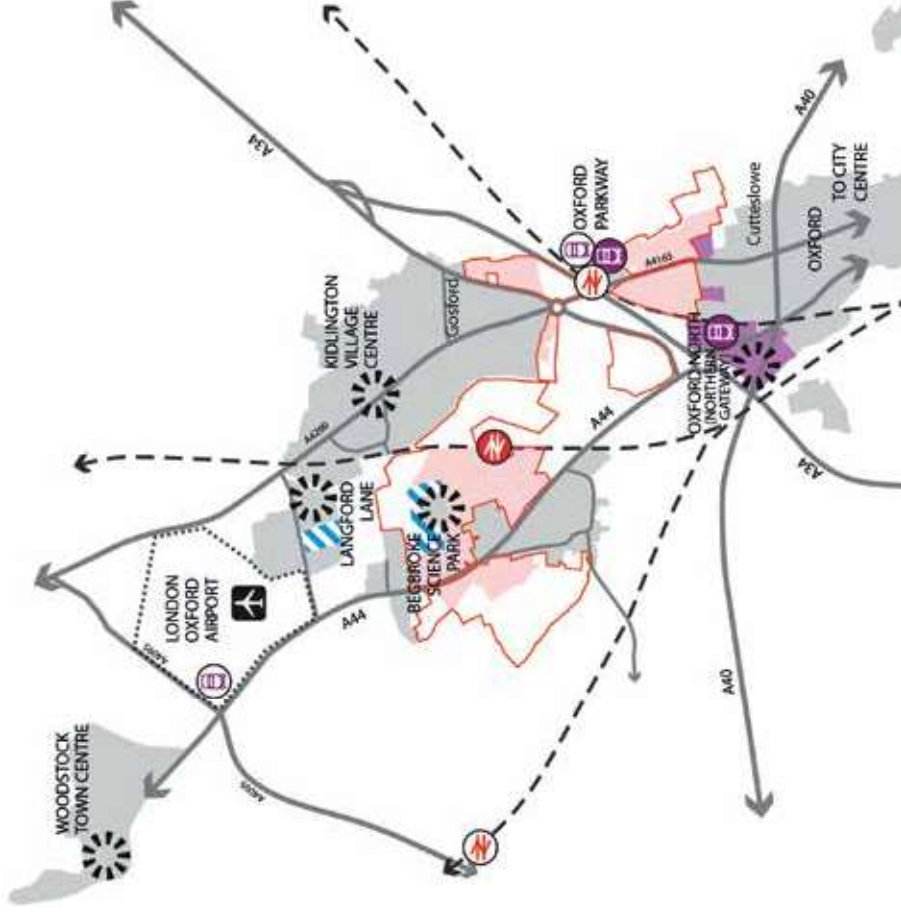


Figure 4: Economic links



2.1.3 Sustainable movement corridors

All sites are located on the major public transport routes of the A44 and A4260/A4165 connecting southern Cherwell to Oxford City and Oxford Parkway station. Significant enhancements to public transport and walking and cycling provision are to be delivered on these routes through the County Council's Local Transport Plan and its strategy for Park & Ride and Rapid Transit. Additional walking and cycling routes are to be created through corridors of green infrastructure including the Oxford Canal corridor.

The emphasis on sustainable modes of travel enables less 'car-centric' movement patterns, promotes active and healthy travel choices and supports inclusion through the provision of convenient, accessible and affordable travel to places of work, recreation and community services.

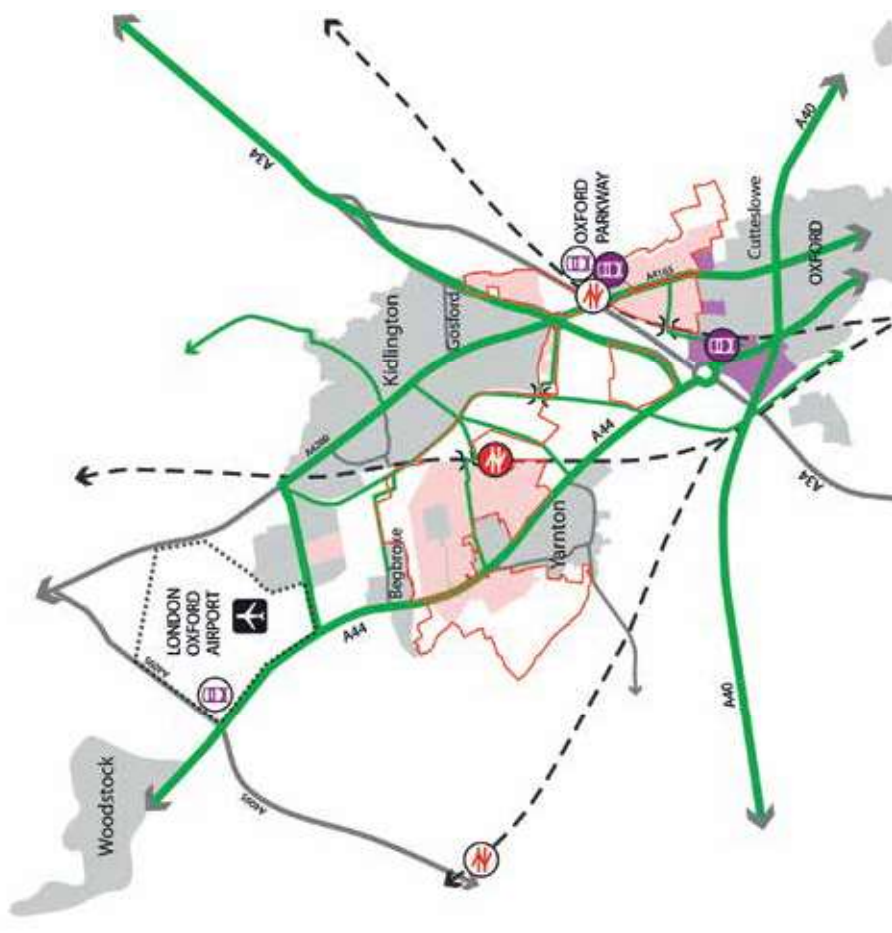


Figure 5: Sustainable movement routes

- Allocation site boundaries
- Proposed growth
- Oxford City Council allocated sites
- Key sustainable movement routes (public transport and/or walking/cycling)
- R Existing rail station
- R Potential new station
- P Existing Park & Ride
- P New or expanded Park & Ride/Transport Hub



2.1.4 Strategic green infrastructure corridors

The sites deliver significant areas of new publicly accessible green infrastructure (GI) and new areas of wildlife habitat which form part of strategic GI corridors:

- ① to the west of Yarnton, Begbroke and Oxford
- ② along the Oxford Canal
- ③ to the east of Oxford and Kidlington/Gosford
- ④ Kidlington towards Oxford

The corridors provide an attractive setting for development and have multiple benefits. They help to maintain separation and distinction between individual settlements; create an appropriate edge and access to the countryside; protect and enhance natural, historic and biodiversity assets; provide corridors for wildlife; and provide leisure and recreation opportunities and walking/cycling routes which encourage health and wellbeing in the existing and new population. Further details of the strategic GI corridors are shown in Appendix 6 of the LPPR.

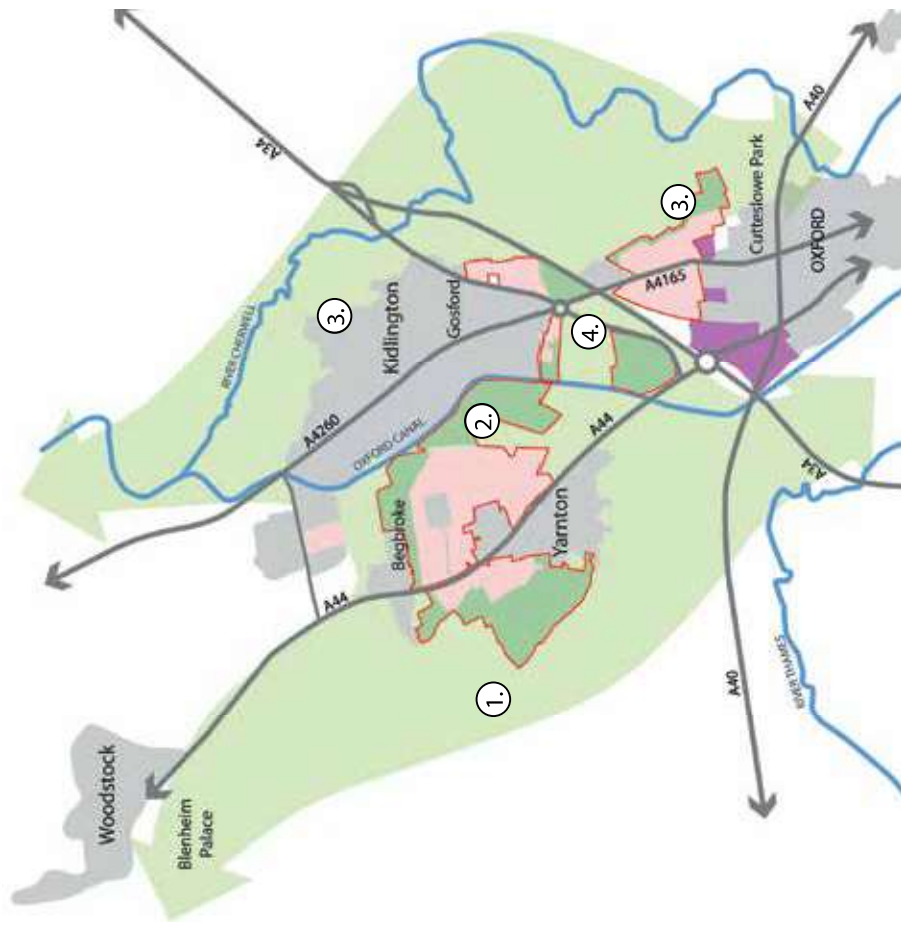


Figure 6: GI corridors

2.1.5 Community services

The planned local provision, through the new housing development, schools (indicative locations for primary school in North Oxford and two primary schools and a secondary school at Begbroke), new local centre facilities (in North Oxford and Begbroke) and formal sports/play areas, provides new facilities which benefit the existing and new population.

Locating facilities within the sites in accessible locations will strengthen Kidlington village centre and will further support and enhance the potential for widespread uptake of walking and cycling for local trips. The location of schools and local centres shown here as in the LPPR has, in some cases, been reviewed through the Development Brief process.

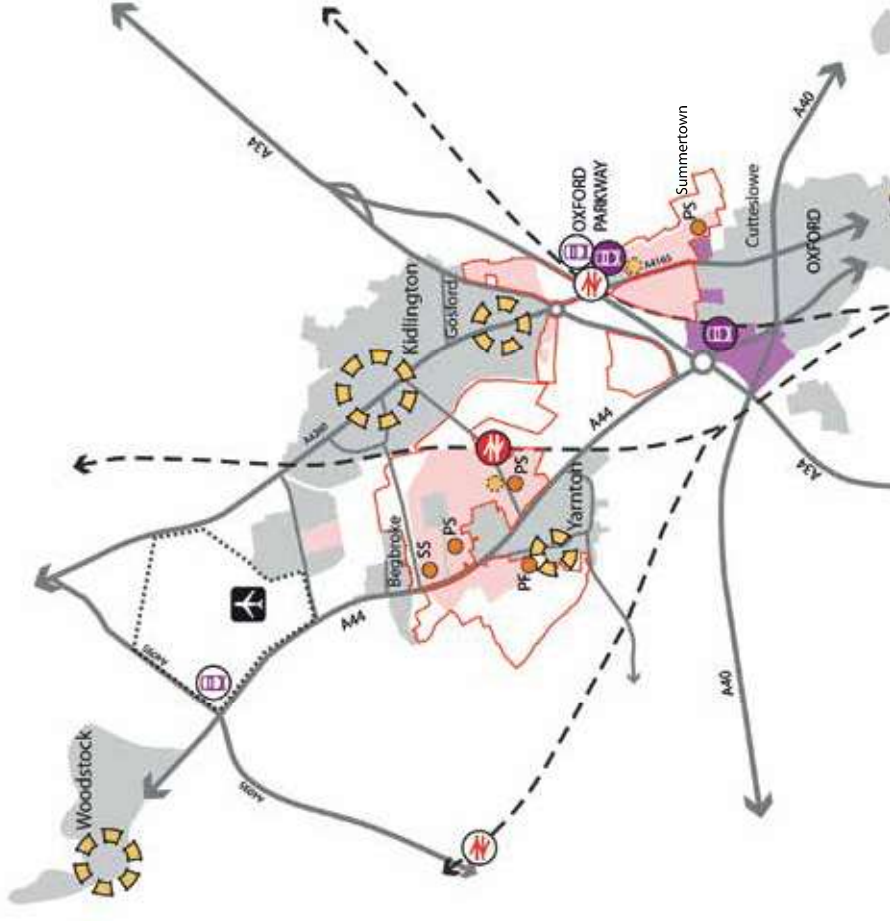


Figure 7: Local centres and schools



## 3.0 Context

### 3.1 The Planning Policy Context

The site subject to this Development Brief - land east of the A44 - is guided by Policy PR8 of the LPPR and its associated Policies Map. In addition to the individual site allocation policy (PR8) the LPPR also contains a number of policies which seek to guide the development of each of the sites and ensure they deliver the homes that are needed, supported by the necessary infrastructure.

Where appropriate, these policies have influenced the content of the Development Brief. In other cases they will need to be followed when planning application(s) are submitted to the Council and all planning applications will be assessed against these policies.

#### 3.1.1 PR8- Landuse Requirements

A new urban neighbourhood on 190 hectares of land to the east of the A44 with the following land use requirements:

- Residential development
  - 1,950 net dwellings on approximately 66 hectares of land
  - Limited number of homes for students and those working for the University at the Begbroke Science Park, to be agreed with the Council
  - 50% affordable housing
- Secondary school on 8.2 hectares of land to incorporate a four court sports hall
- Primary school with three forms of entry on 3.2 hectares of land
- Primary school with two forms of entry on 2.2 hectares of land

- Local centre on 1 hectare of land with provision for:
  - Local convenience retailing (use class A1 – 350-500 sq m net floor space)
  - Ancillary business development (use class B1a only)
  - Financial and professional uses (use class A2)
  - Café or restaurant (use class A3)
  - Community building providing the opportunity for social and childcare facilities
  - Opportunity for required health facilities to be provided
  - Provision for required emergency services infrastructure
- Formal sports and play areas within the developable area
- Local Nature Reserve on 29.2 hectares of land based on Rowel Brook
- Nature Conservation Area on 12.2 ha of land to the east of the railway line, south of the Oxford Canal and north of Sandy Lane
- Public open green space on 23.4 hectares of land as informal canal side parkland
- Retention of 12 hectares of land in agricultural use
- New public bridleways connecting with the existing public right of way network
- Canal Bridge – Land take for a foot, cycle and wheel chair accessible bridge over the Oxford Canal to connect to the allocated site at Stratfield Farm (PR7b).
- Reservation of 0.5 hectares of land for a railway station/halt within the developable area
- Reservation of 14.7 hectares of land for the potential expansion of Begbroke Science Park

The landuse requirements have been included in the brief for reference only.

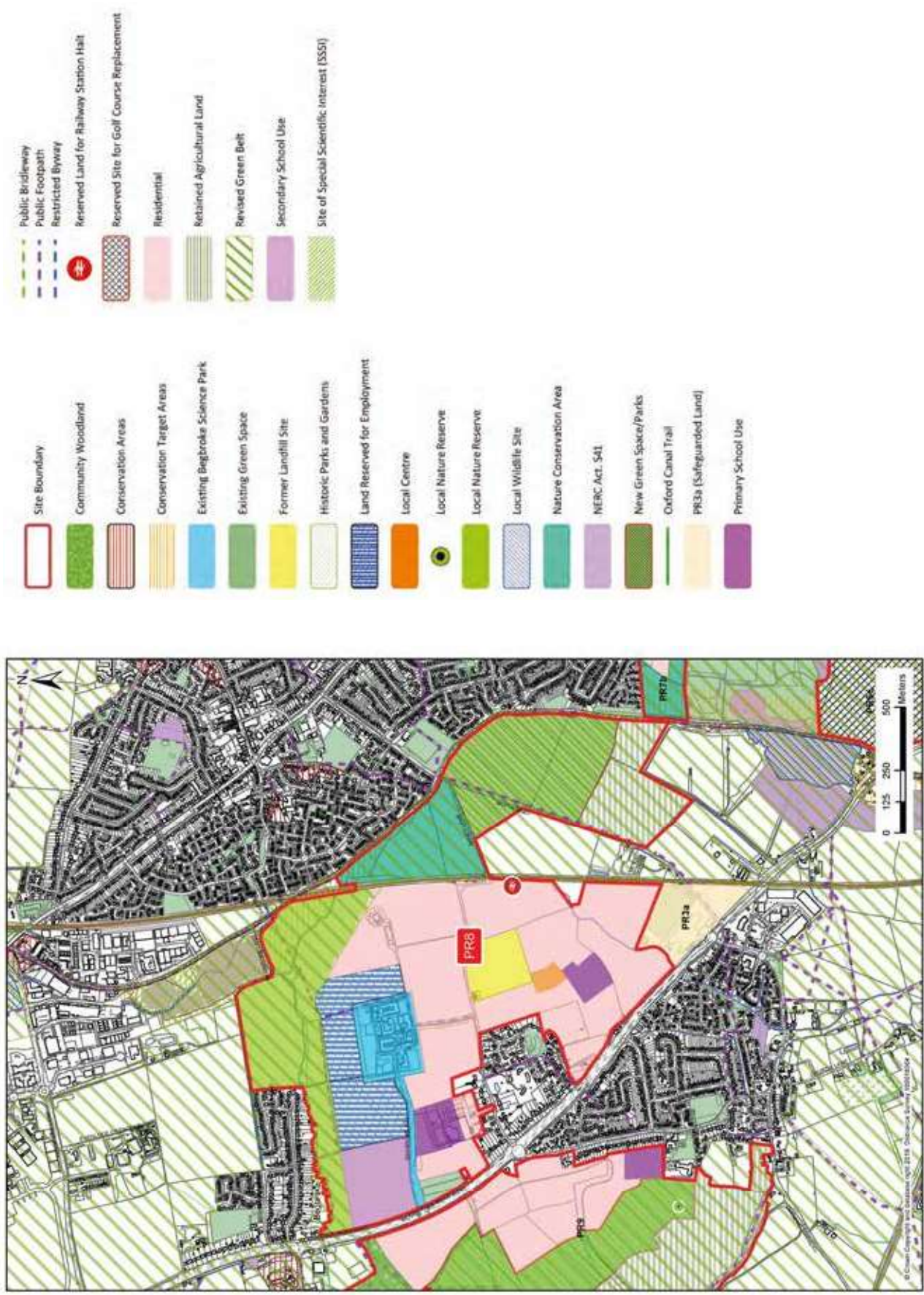


Figure 8: PR8 Policies Map LPPR

Development Brief PR8 / August 2023

### 3.1.2 Submission of Planning Applications

Applications for planning permission for housing in Cherwell to meet Oxford's unmet housing needs will be considered having regard to the policies of the Development Plan and other material considerations such as the National Planning Policy Framework, Planning Practice Guidance and National Design Guide.

The Council will need to assess whether or not development proposals meet the vision, objectives and policies of the LPPR and any other relevant policies from other parts of the Development Plan. This Development Brief is a material planning consideration. See Section 1.2 which explains the status of the Development Brief.

Other material considerations will include relevant Supplementary Planning Documents (SPDs). A list of relevant policy and guidance that has informed this Development Brief is provided at Appendix A.

Further guidance on the submission of planning applications is given in section 7.0 of this Development Brief.



View west along Yarnnton Lane

### 3.2 The Site Context

This section provides a brief overview of the site and its context.

- #### 3.2.1 Location and Size
- The site comprises 190 hectares of land.
  - To the north west, the site is bounded by modern 1-2 storey detached houses in Begbroke.
  - To the north/north-east, the site is bounded by farmland and the Rushy Meadow SSSI, beyond which is the Langford Locks Industrial Estate (which contains a mix of commercial uses) and London Oxford Airport.
  - To the east, the site is bounded by the Oxford Canal and the Lower Cherwell Valley Conservation Target Area beyond which lies the village of Kidlington.
  - A railway line crosses the eastern part of the site in a north-south direction.
  - To the west, the site is bounded by modern semi/detached houses comprising the western extent of Yarnton and the A44. A petrol station is located on the A44 frontage. Yarnton village comprises a mix of housing types and styles with the majority dating from the 20th century and 2-3 storeys high. The small historic core of Yarnton is located around the Church Lane/Rutten Lane junction to the south west of the site.
  - To the south, the site is bounded by Littlemarsh Playing Field containing a cricket pitch and football pitch, and a disused Sewage Treatment Works which lies adjacent to NERC Act S41 grassland habitat just outside the site boundary and is likely to be a valuable habitat (which should be confirmed by additional survey).
  - Rowel Brook crosses the northern part of the site.
- #### 3.2.2 Topography
- The site falls gradually from west to east towards the Oxford Canal and towards the Rowel Brook in the north.
- #### 3.2.3 Existing Land Uses and Services/Facilities
- The majority of the site is in agricultural use with some associated farm buildings.
  - The University of Oxford's Begbroke Science Park, is located towards the centre of the site, north of Sandy Lane and has a mix of 2-3 storey light industrial/research and development and academic buildings. It includes the Grade II listed Begbroke Hill Farmhouse.
  - Yarnton Home and Gardens is positioned in the north western part of the site with an existing access off Sandy Lane.
  - There are allotments adjacent to Woodstock Road (A44) in the north western part of the site.
  - The site contains a former landfill site that can be accessed off Sandy Lane.
- #### 3.2.4 Existing Access and Movement Network
- Sandy Lane, a vehicular route running from east to west, bisects the site and connects Kidlington to Yarnton.
  - Sandy Lane has a weight limit at the narrow canal bridge (which is under signal control) which means HGV access must come from the west
  - Yarnton Lane runs south east to north west through the eastern part of the site but is only partially open to traffic.
  - There are three surface level crossings of the rail line - Roundham Locks, Yarnton Lane and Sandy Lane. Only Sandy Lane level crossing falls within the site's boundary.

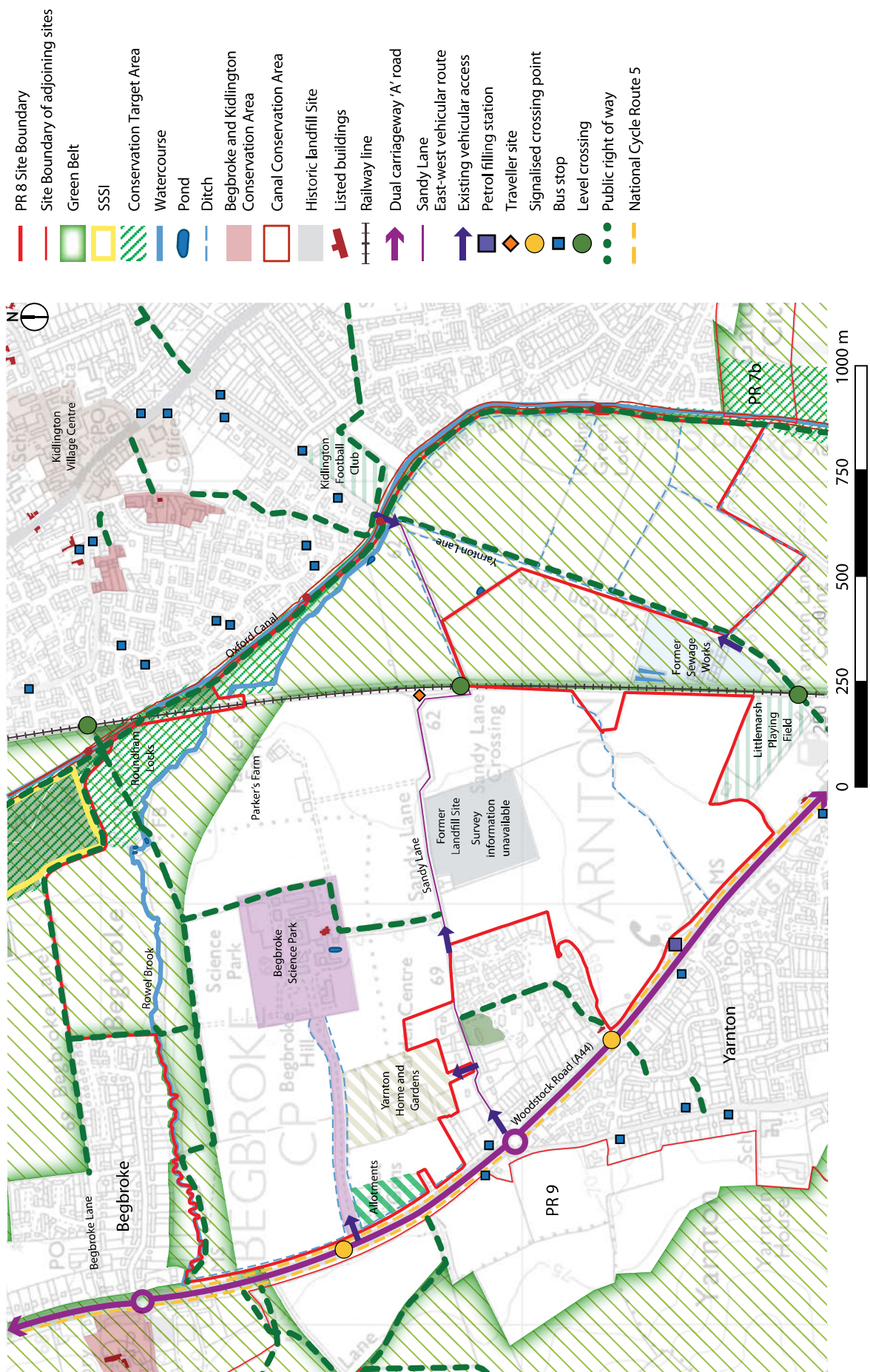


Figure 9: Site Context

- The site can currently be accessed by vehicles via:
  - the existing access road to Begbroke Science Park,
  - Sandy Lane from the A44 and east of the Oxford Canal (Yarnton Road)
  - the existing access to Yarnton Home and Gardens, which serves a small part of the site.
  - the limited access via Yarnton Lane
- Non vehicular access is via:
  - a network of footpaths that cross the northern part of the site.
  - a towpath that runs alongside the Oxford Canal at the eastern boundary of the site.
  - a footpath that runs along the Yarnton Lane.
- The National Cycle Network Route 5 runs along Woodstock Road (A44).
- There are two signalised crossing points on Woodstock Road.
- The site is located between two important public transport corridors A44 and A4260, and served by bus stops which are located along Woodstock Road (A44). There are frequent bus services on these corridors serving Kidlington, Begbroke, Yarnton and towards Oxford (including the S3, S4, 7/700 services).

### 3.2.5 Development Proposals in Surrounding Areas

- Land west of A44 (PR9) lies opposite the site on the western side of the A44.
- Land east of Oxford Canal (PR7b) lies to the eastern side of the site.
- Oxfordshire County Council's proposed Park and Ride site is located to the north at London Oxford Airport.



Draft



View of the Oxford Canal at the eastern boundary of PR8 and the houses of Kidlington beyond

## 4.0 Site Appraisal

### 4.1 Site Constraints

- There is a network of hedgerows across the site containing a wide range of native shrub species. All of the hedgerows are identified as Habitats of Principle Importance (HPI). **(1)**
- Six ponds are present on the site indicated as P1-P6. The presence of Great Crested Newt makes pond P4 at Begbroke Science Park a HPI. **(2)**
- The site contains a single veteran English oak (T1) to the east of the Woodstock Road. **(3)**
- A number of transitional veteran trees (T2,T3, T4 - Willows) are present on the site. These trees provide habitat features and have the potential to become important veteran trees in the future. **(4)**
- A corridor of broad-leaved semi-natural woodland, along the Rowel Brook in the north, conforms to HPI status.
- A tree survey has identified moderate quality trees along the Oxford Canal and Rowel Brook, and internal tree groups along the field boundaries and along Yarnton Lane. **(5)**
- The site contains Badger sets (not shown on plan).
- There is evidence of Water Vole in Rowel Brook and in pond P1.
- The site contains the following land uses:
  - Begbroke Science Park
  - Allotments
  - Yarnton Home and Garden **(6)**

- The site is located in an area of considerable archaeological interest and a number of sites recorded with cropmarks including later prehistoric settlement sites and Bronze Age barrows. An area of late prehistoric settlement has been also recorded to the site.
- Part of the site along the Oxford Canal and Rowel Brook is within modelled flood zones 2 and 3. These areas should be a focus for green infrastructure enhancements. The Strategic Flood Risk Assessment (SFRA) notes that there is the risk of shallow groundwater in places.
- The site contains an ex-landfill site (Sandy Lane – East Gravel Pit). Surveys undertaken on surrounding land show that, subject to mitigation, there are no significant land use constraints relating to ground conditions in the vicinity of the former landfill site. Further surveys are required to inform safe remediation and potential use. **(7)**
- Noise from the airport, railway line and A44 is a potential constraint. A Baseline Noise Survey Report (November 2019) highlights the need for careful consideration of separation between sensitive land uses and noise sources.
- There is a risk from ball strike from Littlemarsh Playing Fields. A ball strike risk assessment will be required. **(8)**
- The Oxford Canal Conservation Area runs along the eastern boundary of the site. This contains a number of listed structures including the Sandy Lane canal bridge and Roundham Lock.
- The site contains one designated heritage asset, this being the Grade II listed Begbroke Hill Farmhouse which is within the Begbroke Science Park complex. In addition, a Grade II listed Tudor Cottage is located on the corner of Gravel Pits Lane and the A44 Woodstock Road. **(9)**

- Other assets in the vicinity include Roundham Lock and Kidlington Green Lock on the canal and the cluster of listed buildings off Spring Hill Road associated with St Philips Priory, St Philips Church and St Michaels Church which sit on the opposite side of the A44 and the Begbroke Village roundabout to the northwest of the site. There is also a Grade II Listed Rose Cottage and The Grapes Inn on the A44 frontage which will require assessment.
- Further afield there are a number of listed buildings in Kidlington and there are buildings and the park associated with Yarnton Manor and other buildings within Yarnton Village. Whilst these buildings are further afield and therefore have limited direct impact, the character and setting form an interesting discussion as to how these can influence character.
- Overhead electric lines and an operational foulwater rising main cross the site.
- There is existing residential development within the site in various locations including the western boundary of the site abutting A44, in the vicinity of the former landfill site and the rail level crossing next to the Sandy Lane.<sup>(10)</sup>
- Oxfordshire County Council's planned transport improvements to the A44 corridor may affect the site's western boundary.
- There is a need to reserve land within the site for possible future development of a railway station/halt.
- Limitations to the use of Sandy Lane for vehicular access.
- Sandy Lane level crossing has poor sight lines on approach from the west and a weight-restricted canal bridge with signalised one-way control (which can cause traffic to queue back towards the level crossing).
- The nearby road network including A4165 (Oxford Road), A44, A34 and A4260 is heavily congested at peak times.

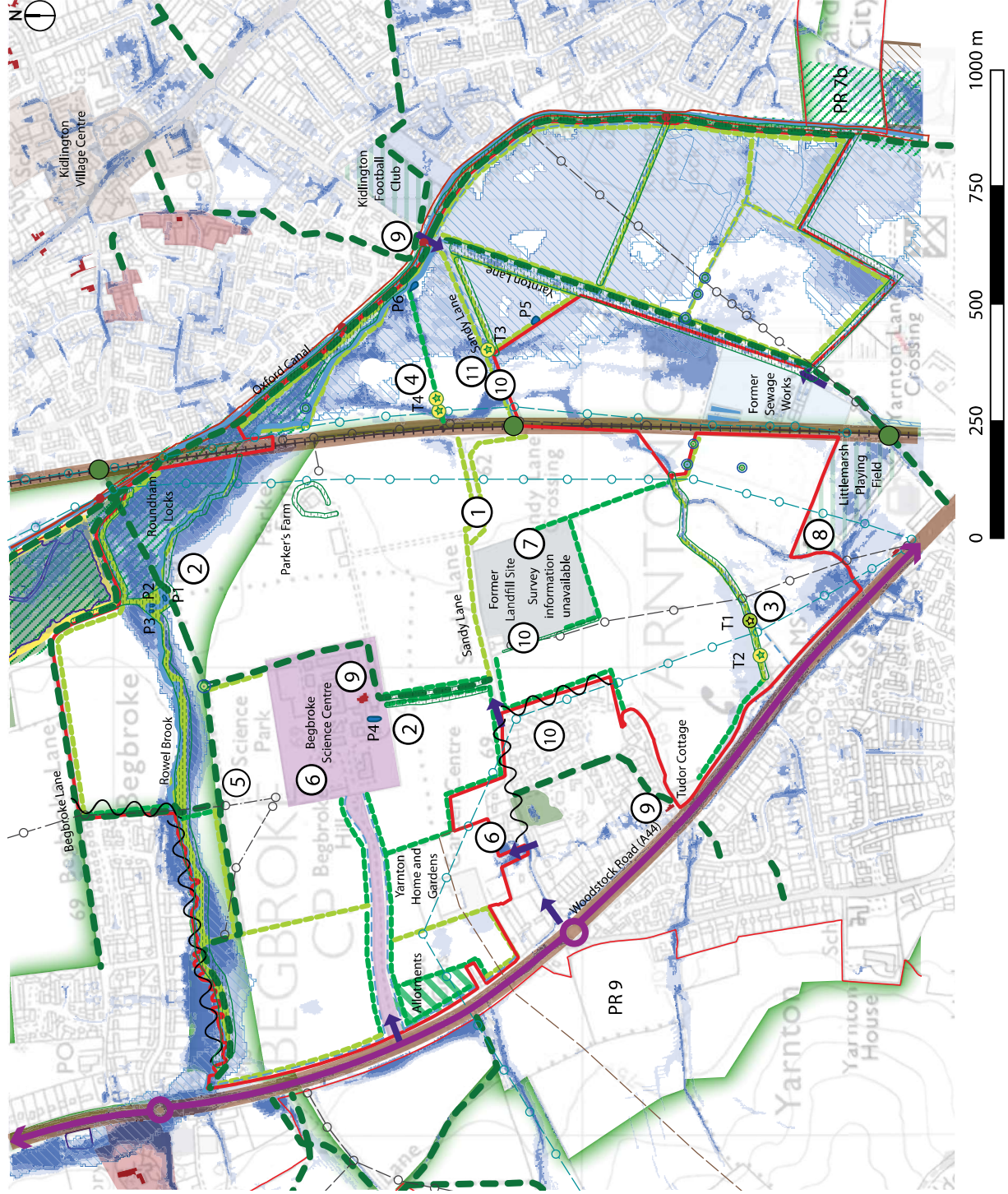
- The site is vulnerable to commuter parking by non-residents and enforcement associated with the sites location.
- The need for access between Parkers Farm and land east of the Railway to enable management of the nature reserves.<sup>(11)</sup>



View of the Grade II listed Begbroke Hill Farmhouse

4.0 Site Appraisal

- PR 8 Site Boundary
- Site Boundary of adjoining sites
- SSSI
- Green Belt
- Priority Habitat Inventory
- NERC Act S41 Habitat
- Conservation Target Area
- Woodland Trust Sites
- District Wildlife Site
- Species-rich hedgerow (HP1)
- Other hedgerow
- Tree considered to be of veteran status
- Tree considered to be of transitional veteran status
- High and moderate quality tree
- Moderate quality group of trees
- Broad-leaved semi-natural woodland
- EA Flood Zone 2\*
- EA Flood Zone 3\*
- 1 in 30 Surface Water Floodrisk\*\*
- 1 in 100 Surface Water Floodrisk\*\*
- 1 in 1000 Surface Water Floodrisk\*\*
- Watercourse
- Pond
- Ditch
- Historic landfill site
- Oxford Canal Conservation Area
- Begbroke, Kidlington Conservation Areas
- Listed buildings
- Railway line
- Dual carriageway - 'A' road
- Existing access
- Public rights of way
- Level crossing
- Housing backs
- HV 11kv overhead line
- EHV 33kv overhead line
- Operational Thames Water FW Rising Main
- Noise pollution requiring mitigation



\* Flood Map for Planning Rivers and Sea Flood Zone 3 and Zone 2 (The Environment Agency, 2020)  
 \*\* Updated Flood Map for Surface Water (The Environment Agency, 2019)  
 Drawing shows national constraints datasets where site surveys are not available

Figure 10: Site Constraints

## 4.2 Site Opportunities

### 4.2.1 Place Shaping

- Opportunity afforded by the scale of the site to establish an innovative new contemporary, urban neighbourhood with a mix of typologies and uses which integrates well with the proposed expansion of the Science Park as set out in Policy PR8, and has strong links towards Kidlington Village Centre, to Oxford, and to the adjacent communities of Yarnton and Begbroke. **(1)**
- Potential for an innovation district with a vibrant community, mix of typologies, an ability to evolve over time, to embrace nature and provide a mix of uses integrated with the neighbourhood.
- Allotments should be retained, replaced with equivalent or extended. **(2)**
- Opportunity to reconfigure Yarnton Home and Garden site to integrate with wider site. **(3)**
- Improved active travel routes and considerable increase of accessible public open and natural space, supporting communities in Begbroke, Yarnton and Kidlington.

### 4.2.2 Heritage and Townscape Character

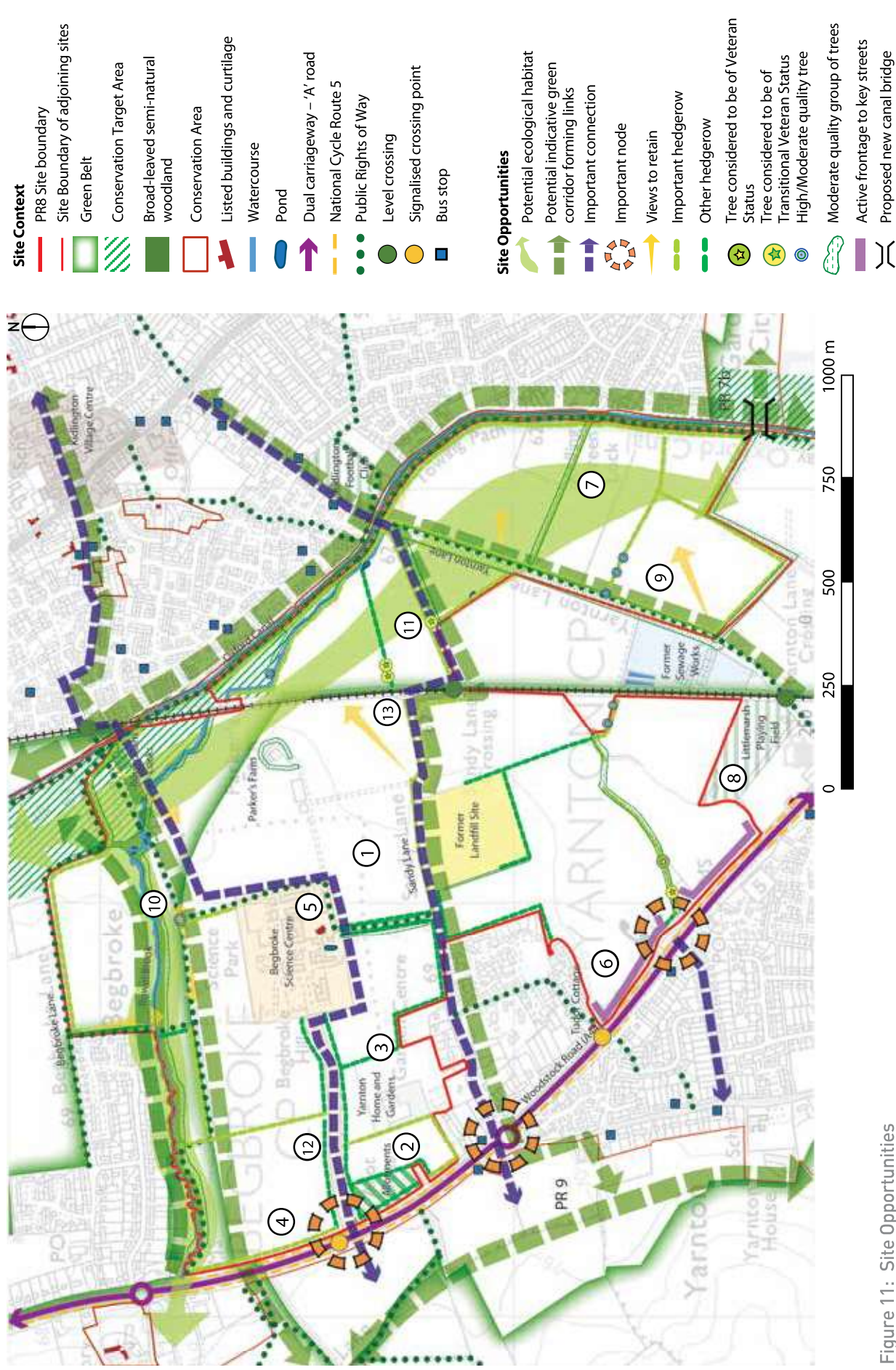
- Opportunity to create a distinctive neighbourhood but at the same time respond appropriately to the landscape, settlement patterns, building typologies and traditional materials of the local area (See Cherwell Design Guide 2.3)
- Opportunity to retain a sense of separation between Begbroke and Yarnton, and set back built development along the A44 to the north of Yarnton. **(4)**
- Opportunity to provide an appropriate design response to enhance the setting of the listed buildings and the adjacent Conservation Areas of Begbroke and the Oxford Canal, in particular, opportunity to better reveal the significance of Begbroke Hill Farmhouse and Tudor cottage. **(5)**
- Opportunity to create an attractive development frontage to the A44 and Begbroke Science Park road. **(6)**

### 4.2.3 Views and Sightlines

- Opportunity to capture and retain views into and out of the site including:
  - To the west towards higher land at Begbroke Wood and Bladon Heath
  - To the east towards the Oxford Canal
  - To the north towards Roundham Locks and Rowel Brook

### 4.2.4 Landscape Character

- Opportunity to provide a connected corridor of green infrastructure across the site, connecting with PR9 and PR7b. This should incorporate Rowel Brook in the form of a publicly accessible Local Nature Reserve, the Oxford Canal corridor, a Nature Conservation Area where access is restricted to a fenced route and an informal canal-side parkland, in line with Policy PR8. **(7)**
- Opportunity to expand and connect with Littlemarsh Playing Fields. **(8)**
- Opportunity to incorporate retained veteran/transitional veteran, high and medium value trees, important hedgerows and ponds to create a mature landscape structure for the development
- Opportunities to retain and enhance existing habitats and achieve a net gain in biodiversity through the creation of a extensive areas of new / improved habitats including:
  - restoration of Water Vole and Otter habitat along the canal edge.
  - management of retained agricultural land in the south-east part of the site for farmland birds to contribute towards compensation for loss of farmland. **(9)**
  - creation of Lowland Meadow Grassland with hedgerows on either side of Rowel Brook. **(10)**
  - enhancement of grasslands within the proposed Nature Conservation Area. **(11)**
- There is an opportunity to provide an area for dog walking and recreation within the site that is away from SSSI, Rowel Brook, ponds and Oxford Canal to limit visitor impact on these habitats.
- Opportunities to remediate the former landfill site and use as a green space and wildlife stepping stone within the development.



#### 4.2.5 Movement and Access

- Opportunity to strengthen connectivity with Begbroke to the north and south and towards Yarnton
- Opportunity for vehicular access to the site from:
  - the existing vehicle access to Begbroke Science Park <sup>(12)</sup>
  - new access points onto the A44
  - the existing Yarnton Home and Garden access
- Opportunity to provide public transport access through the site via buses linking to the bus routes on A44 and the proposed park & ride at London Oxford Airport.
- Opportunity for an enhanced network of walking and cycling connections across the site towards:
  - Kidlington village centre and facilities via existing canal bridges and Sandy Lane
  - London Oxford Airport and Langford Lane employment sites to the north including via upgrade to the canal towpath
  - Begbroke and Yarnton Lanes which abut the site
  - Littlemarsh playing field to the south
  - Across A44 to Yarnton village and to Land West of Yarnton (PR9), south to Oxford Northern Gateway and City Centre.
  - Oxford and Oxford Parkway station via Sandy Lane, and across the Oxford Canal via a new pedestrian, cyclist, wheelchair accessible canal bridge connecting to Land at Stratfield Farm (PR7b), to south Kidlington including Sainsbury's roundabout.
  - Along the Rowel Brook connecting to Roundham Locks
  - Towards National Cycle Route 5 along the A44
- Opportunity for upgrade to canal towpath for walking and cycling. The canal towpath has recently been upgraded to a pedestrian/cycle quiet active

travel route between Oxford City Centre and up to where the A44 passes over the canal. This treatment should be extended up through the site as far as possible, ideally to Langford Lane to provide a direct, traffic free route to the businesses there and the Airport.

- Opportunity for innovative and high quality design solutions to promote sustainable travel. It will be important to make best use of existing and planned infrastructure, provide new or improved infrastructure, reduce the need to travel and support walking, cycling and public transport.
- Opportunity to reserve land for a possible future rail station/halt serving the site and the wider Kidlington area.
- Opportunity for the northern alignment of the Sandy Lane crossing and restricted vehicular access route. Subject to Network Rail and OUD reaching an agreement. <sup>(13)</sup>
- Opportunity for new bus routes through the site and/ or strengthened connections to high quality bus routes along the A44.
- Opportunity to limit vehicular traffic through Sandy Lane to achieve a green link, between Begbroke/Yarnton and Kidlington, meet the interests of railway safety and help meet sustainable transport objectives of growth along the A44.
- Opportunity to help address parking and enforcement issues associated with sites location.

## 5.0 Vision and objectives

### 5.1 Vision

In response to the site's local surrounding context and constraints, the vision for Land East of the A44 has gradually evolved to affirm the design opportunities available to meet the objectives of the LPPR and is described below. The vision is further developed by the Design Principles contained in this document which set out the detailed requirements.

*The expansion of the University's Begbroke Science Park will be integrated within a distinctive, urban neighbourhood featuring innovation and high levels of sustainability, which will become recognised as a national placemaking exemplar. The new neighbourhood will have walking and cycling at its core, by creating an environment where the private car is a guest whilst catering for public transport. It will be publicly accessible, well connected by footpaths and cycleways to the surrounding neighbourhoods, existing local services and enhanced public transport links on the A44.*

*The development will be a distinct and unique blend of green space, water environments and natural spaces promoting good health habits. A new local centre, schools and green infrastructure corridors adjacent to the Oxford Canal will link existing communities with publicly accessible open spaces of high quality wildlife and biodiversity.*

*It will be successfully connected to Begbroke, Yarnton and Kidlington which will have retained their own identities.*

The land east of Yarnton is to be developed following the guidance contained within this document and in line with the policies of the Adopted Cherwell Local Plan 2011-2031 (Part 1), Cherwell Local Plan Partial Review 2011-2031 (Part 1) Partial Review – Oxford's Unmet Needs, guidance in the Cherwell Residential Design Guidance (2018), The Setting of Heritage Assets Historic Environment Good Practice Advice in Planning Note 3 (Second Edition) by Historic England (2017) and other relevant national and local policy and guidance. Key relevant local policies and guidance are listed at the end of each section of this chapter and the Development Principles chapter (chapter 6) although all relevant policies, including those not listed, should be responded to. In particular, the development should meet the requirements set out in Partial Review Plan Policy PR8 (see chapter 3.0 for details).

In summary, key delivery requirements under Policy PR8 are:

- 1,950 homes on approximately 66 hectares of land seeking 50% affordable housing provision across the site to meet Oxford's housing needs
- secondary school and four court sports hall on 8.2 hectares of land
- primary school with three forms of entry on 3.2 hectares of land
- primary school with two forms of entry on 2.2 hectares of land
- local centre on one hectare of land
- reservation of 14.7 hectares of land for Begbroke Science Park expansion
- Local Nature Reserve on 29.2 hectares of land
- nature conservation area on 12.2 hectares of land
- public open space on 23.4 hectares of land
- retention of 12 hectares of agricultural land
- reservation of 0.5 hectares of land for a future railway halt.
- provision of a foot, cycle and wheel chair accessible bridge over the Oxford Canal linking the site with land at Stratfield Farm (Policy PR7b)
- facilities for formal sports, play areas and allotments



It is the Council's preference that in lieu of on-site formal sport pitch provision an appropriate financial contribution be made towards new and improved facilities at south east Kidlington, based upon CDC adopted developer contribution standards. Informal play and amenity space will still be required to be provided within the built-up area to adopted standards.

Figure 12 illustrates the development framework for the site. This is a flexible framework to enable innovative design to be brought forward in line with the Vision and requirements of Policy PR8. Alternative land use arrangements will be considered through future planning applications subject to evidenced justification and would be designed in consultation with CDC.

Design principles which underpin the delivery of the development framework are set out in the next chapter.



5.0 The Vision for the Site

- Site Boundary
- Site Boundary of adjoining sites
- Primary land use
  - Residential
  - Employment
  - Employment or residential
  - Primary school\*
  - Secondary school\*
  - Mixed use – retail, commercial, residential
    - 1. Retained Yarrton Home & Garden
    - 2. Local centre
    - 3. Convenience facilities
  - New public green space/parks/wildlife areas
  - Proposed green spaces within developable area\*\*
  - Local Nature Reserve
  - Nature Conservation Area (limited public access)
  - Revised Green Belt
  - Retained Agricultural Land
  - SSSI
  - NIERC Act. 541
  - Priority Habitat Inventory
  - Broad-leaved semi-natural woodland
  - Retained hedgerows
  - Reinstated hedgerow
  - Tree considered to be of veteran status
  - Tree considered to be of transitional veteran status
  - Other retained trees
  - Retained group of trees
  - Soft edge
  - Ditch retained and integrated into site drainage
  - Equipped play area (indicative location)
  - Parkland/visitor hub/play space
  - Noise attenuation (size/type to be confirmed)
  - Vehicular access (southern location is indicative)
  - Proposed A44 pedestrian/cycle crossing
  - National Cycle Route 5
  - Existing/improved PROW/cycle routes
  - Key new walking/cycling routes
  - Zone for potential rail halt
  - New pedestrian/cycle bridge over railway/canal
  - Potential for alternative bridge further north with restricted vehicular access, subject to agreement between OUD and Network Rail

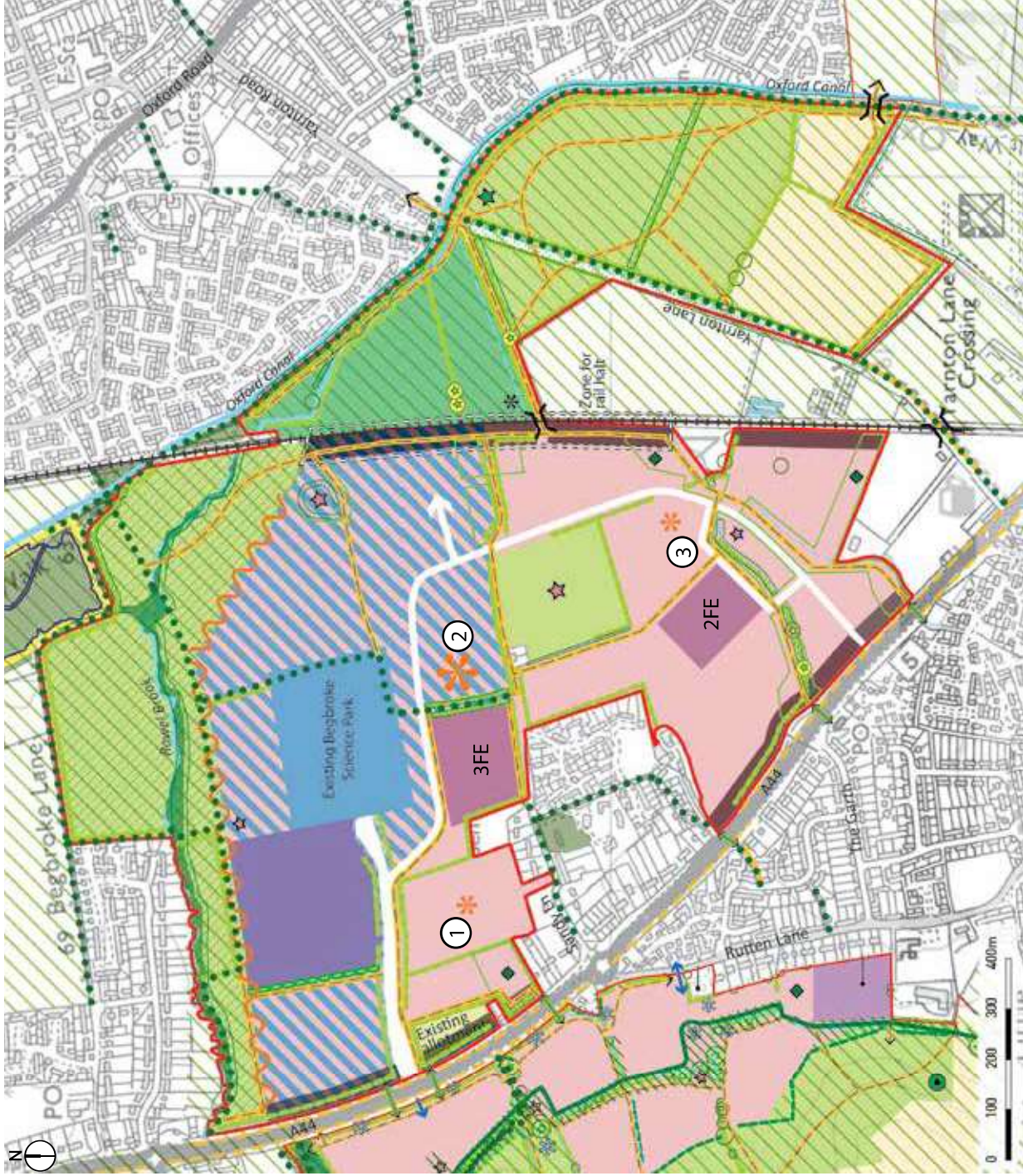


Figure 12: Development framework

\*School site location subject to further detailed assessment  
 \*\*Built-up area to incorporate amenity space as per adopted standards

## 6.0 Development Principles

### 6.1 Sustainable Construction and Energy Efficiency

The development is to comply with and where possible exceed the local and national standards for sustainable development including enhancement to the natural environment and biodiversity net gain. This includes mitigating and adapting to climate change, increasing local resource efficiency, minimising carbon emissions, promoting decentralised and renewable or low carbon energy and ensuring that the risk of flooding is not increased and where possible, reduced.

The detailed layout of the development will need to encourage the sustainable and safe management of waste in each individual household while minimising visual and pollution impacts. CDC would welcome innovative approaches to delivering sustainable development on the site.

The use of recycled materials in the construction of the development and consideration of the Circular Economy is supported.

Construction Exclusion Zones and haulage routes are to be incorporated into the build programme in order to protect the site's green infrastructure and topsoil resource. Topsoil is to be managed in accordance with the Construction Code of Practice for the Sustainable Use of Soils on Construction Sites 2009 (CCoP) published by the Department of Environment, Food and Rural Affairs (Defra).

#### **Cherwell Local Plan 2011-2031 (adopted 20 July 2015)**

Policy ESD 1: Mitigating and Adapting to Climate Change  
 Policy ESD 2: Energy Hierarchy and Allowable Solutions  
 Policy ESD 3: Sustainable Construction  
 Policy ESD 4: Decentralised Energy Systems  
 Policy ESD 5: Renewable Energy  
 Policy ESD 6: Sustainable Flood Risk Management  
 Policy ESD 7: Sustainable Drainage Systems  
 Policy ESD 8: Water Resources  
 Policy ESD 15: The Character of the Built and Historic Environment

#### **Cherwell Residential Design Guide SPD (adopted 16 July 2018)**

Chapter 4: Establishing the Structuring Principles  
 Chapter 7: Building Elevations and Details  
 Chapter 8: Innovation and Sustainability

## 6.2 Healthy Place Shaping

Healthy place shaping is a strategic priority for both Oxfordshire's Health & Wellbeing Board and the Future Oxfordshire Partnership (formerly the Oxfordshire Growth Board) which is using the Oxfordshire Housing & Growth Deal to embed healthy place shaping in the planning process, especially in light of emerging evidence from local and national experience of Healthy New Towns (including the initiatives at Barton Park and Bicester Healthy New Towns) and the significant positive impact on health and well-being. This is reflected in the guiding principles of the Oxfordshire Strategic Vision.

This early planning and provision of health promoting design and infrastructure, such as community facilities, green spaces and safe and legible walking and cycling routes for recreation and commuting, has been shown to be important in influencing and establishing positive behaviour, healthier life-style habits and distinct and cohesive communities which are also well connected to existing settlements.

The site will be developed in a way which contributes to healthy living and the well-being of its local residents and visitors. It will:

- provide new and enhanced walking, wheelchair and cycling connections which support active lifestyles at any age and which prioritise pedestrians and cyclists over the car;
- improve and enhance connectivity to the existing public rights of way, National Cycle Route 5 and existing facilities at Begbroke, Kidlington and Yarnton, and to existing and potential public transport services;
- provide connections with PR7b site and Oxford Canal, and Kidlington and Oxford;
- provide new bus stops within the site and the potential for a new rail halt;
- provide local facilities including a local centre, primary and secondary schools and employment opportunities within walking distance of residents;
- create significant areas of new accessible public open space, a local nature reserve, a nature conservation area, food growing opportunities and children's play space;

- meet the need for early provision of health promoting infrastructure;
- provide facilities for storage of bicycles and measures to promote their use (e.g. changing facilities);
- provide flexibility to allow for innovation in sustainable travel choices (e.g. e-bikes and e-scooters);
- meet high quality design standards as specified in Building for a Healthy Life.

The Health Impact Assessment commissioned for the Oxfordshire Authorities has been developed as an HIA proforma/toolkit and methodology to be applied to local plans and major developments in the county to achieve a consistent approach. The toolkit was published in 2021 (see link below).

The development of the site should comply with policies that promote the creation of healthy communities including those listed below.

### **Cherwell Local Plan 2011-2031 (adopted 20 July 2015)**

- Policy BSC 7: Meeting Education Needs
- Policy BSC 8: Securing Health and Well-Being
- Policy BSC 9: Public Services and Utilities
- Policy BSC 10: Open Space, Outdoor Sport and Recreation Provision
- Policy BSC 11: Local Standards of Provision- Outdoor Recreation

### **Oxfordshire Health Impact Assessment Toolkit, 2021:**

<https://futureoxfordshirepartnership.org/wp-content/uploads/2021/01/210126-Oxon-HIA-Toolkit-FINAL.pdf>

### 6.3 Character and Layout

The scale of the PR8 site provides the opportunity to create a distinctive, higher density urban village which is contemporary in character, while being sensitive to the setting of the surrounding villages of Begbroke, Yarnton and Kidlington. It is to create an attractive frontage to the A44 on the approach to Oxford supporting a change in character away from a highways dominated environment and creating crossing opportunities. The contrast between the dense urban development and canal-side parkland setting will be used as a positive and integral design feature. Green walking, cycling and wheelchair user corridors will be created running north-south and east-west through the site providing connections between characterful spaces for play and recreation and linking Sandy Lane and the canal-side parkland/public open space. These green corridors will connect the development with built-up areas of Yarnton, Begbroke and Kidlington.

The development brief site-specific vision, development principles and 'parameter plans' have been prepared to ensure a comprehensive development in compliance with Plan policies. Planning applications should be in accordance with the brief.

The development is to follow the general design approach to masterplanning set out in the Cherwell Residential Design Guide.

#### **Development principles:**

- In line with Policy PR8 a single comprehensive, outline scheme is to be approved for the entire site supported by a Delivery Plan across the multiple planning applications which the Council expects to be submitted. It is recommended that a design code is prepared for the entire site as a means of providing additional design co-ordination and quality management across several phases of development.
- The development is to establish a distinctive urban character with an emphasis on innovation, sustainability and science, drawing on national and international precedents. This is to be reflected in bespoke and exemplary architecture, public realm and green infrastructure design. Generic suburban house types and highways dominated layouts will not be accepted.
- Modern architecture is to take a respectful approach and inspiration from the traditional vernacular and local materials of the surrounding village conservation areas.
- The development is to have a street-based layout, which is publicly accessible and connected with the surrounding street and public rights of way network.
- The development is to provide a legible hierarchy of streets and spaces, with urban form and massing varying in response to the proposed character areas, local setting and landscape and areas of high activity – such as the local centre, primary schools, Science Park and secondary school which form focal points within the neighbourhood.
- The horizontal and vertical mixing of uses is supported, with the aim of creating a vibrant, mixed use environment. Flexibility in the arrangement of employment and housing use allocations is indicated on the development framework to support the creation of an integrated mixed use environment.

- The existing and expanded Begbroke Science Park, allotments on the A44, and former landfill site and existing residential dwellings are to be well integrated into the overall layout.
- Existing allotments can be relocated if necessary, in addition to provision of allotments to adopted standards, subject to clear justification and consideration of the impact on existing users and residents of Begbroke and Yarnton.
- The site layout is to be co-ordinated with proposals for site PR9 to the west of the A44 and PR7b to the east of the Oxford Canal in relation to the provision of access junctions, A44 frontage, green infrastructure corridors and pedestrian and cycling connectivity. The Council has adopted development briefs for these sites which should be referred to.
- Green infrastructure within the site, including hedgerow and drainage corridors, is to be designed as a connective element which supports the movement of wildlife and encourages walking and cycling and community use.
- Mixed uses, local centre and Science Park are to overlook the open green spaces wherever possible to allow for visual connectivity and encourage passive surveillance of those spaces.
- Proposed housing must meet the 'Technical housing standards – nationally described space standard (2015)'.
- The affordable housing tenure and mix is to be agreed with Cherwell District Council. There is a preference for social rent tenure in line with Oxford City Council policy.
- Affordable housing and housing aimed at specific groups (such as University staff and students) is to be carefully distributed across the development to avoid the creation of segregated communities.

The development will create three distinct but complementary areas of character. There are two mixed use character areas described in this section located to the north and south of Sandy Lane. The third character area, is in reference with the existing Canalside green corridor along the eastern edge of the site.

- Science and learning cluster
- Innovation village
- Canalside green corridor

Each area is described in more detail below. Figure 13 provides an overview of the development site character areas. Figure 14 provides further detail on urban design considerations including key frontages and building heights.

- Site Boundary
- Site Boundary of adjoining sites
- Science and learning cluster
- Innovation village
- Canalside green corridor

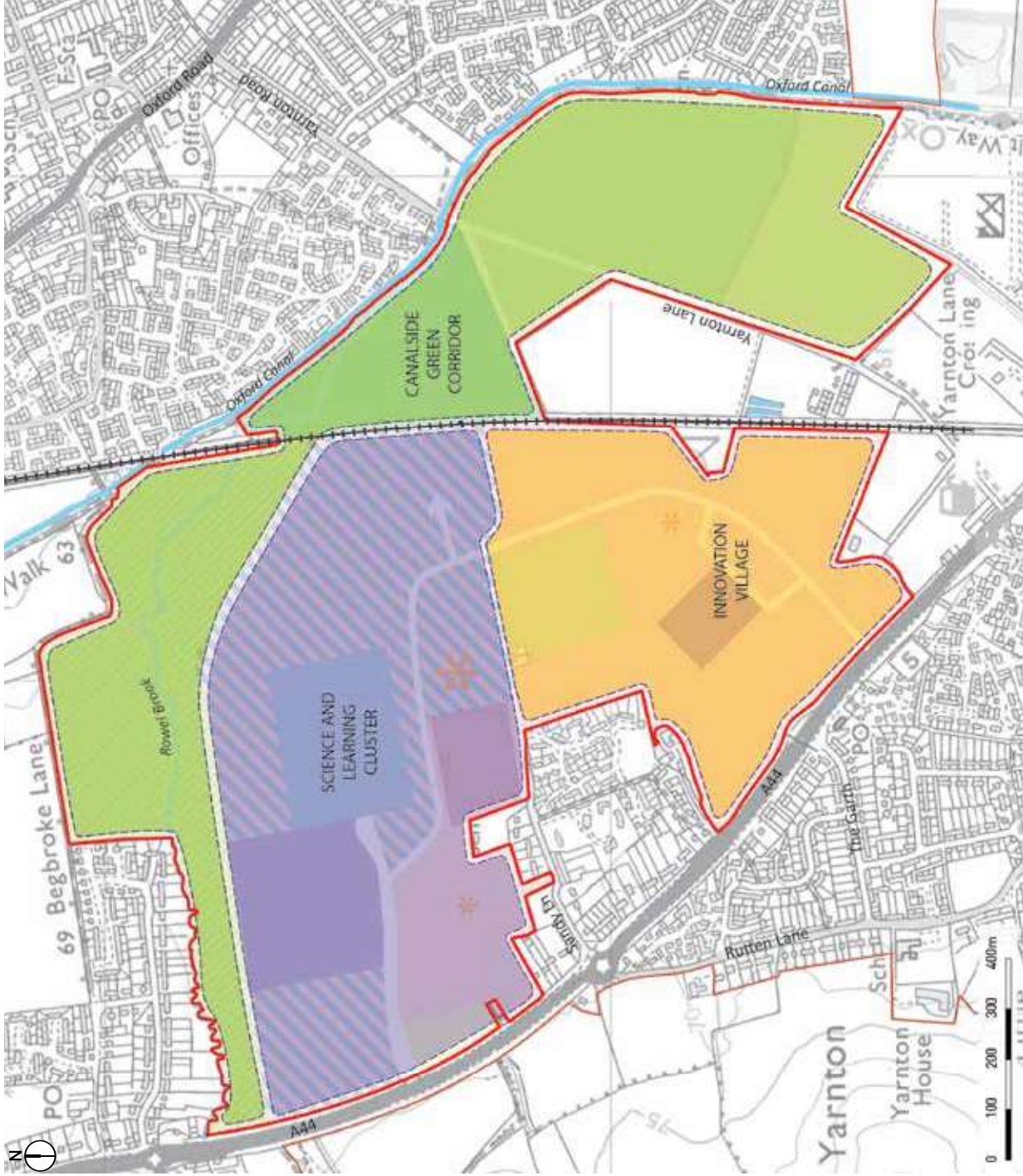


Figure 13: Character areas

6.0 Development Principles

- Site Boundary
- Site Boundary of adjoining sites
- 3-5 storey
- 2-4 storey
- 2-3 storey
- Key frontages (indicative)
- School entrance plaza
- Public square
- Open aspect towards green corridor
- Sensitive edge with existing residential
- New public green space/parks/wildlife areas
- Local Nature Reserve
- Nature Conservation Area (limited public access)
- Retained Agricultural Land
- Broad-leaved semi-natural woodland
- Oxford Canal Conservation Area
- Retained hedgerows
- Reinstated hedgerow
- Tree considered to be of Veteran Status
- Considered to be of Transitional Veteran Status
- Other retained trees
- Retained group of trees
- Ditch retained and integrated into site drainage
- Noise attenuation (size/type to be confirmed)
- Proposed A44 pedestrian/cycle crossing
- National Cycle Route 5
- Existing/improved PRow/cycle routes
- Key new walking/cycling routes
- Improved active travel routes
- Zone for potential rail halt
- New pedestrian/cycle bridge over railway/canal
- Potential for alternative bridge further north with restricted vehicular access, subject to agreement between OUD and Network Rail
- Listed building

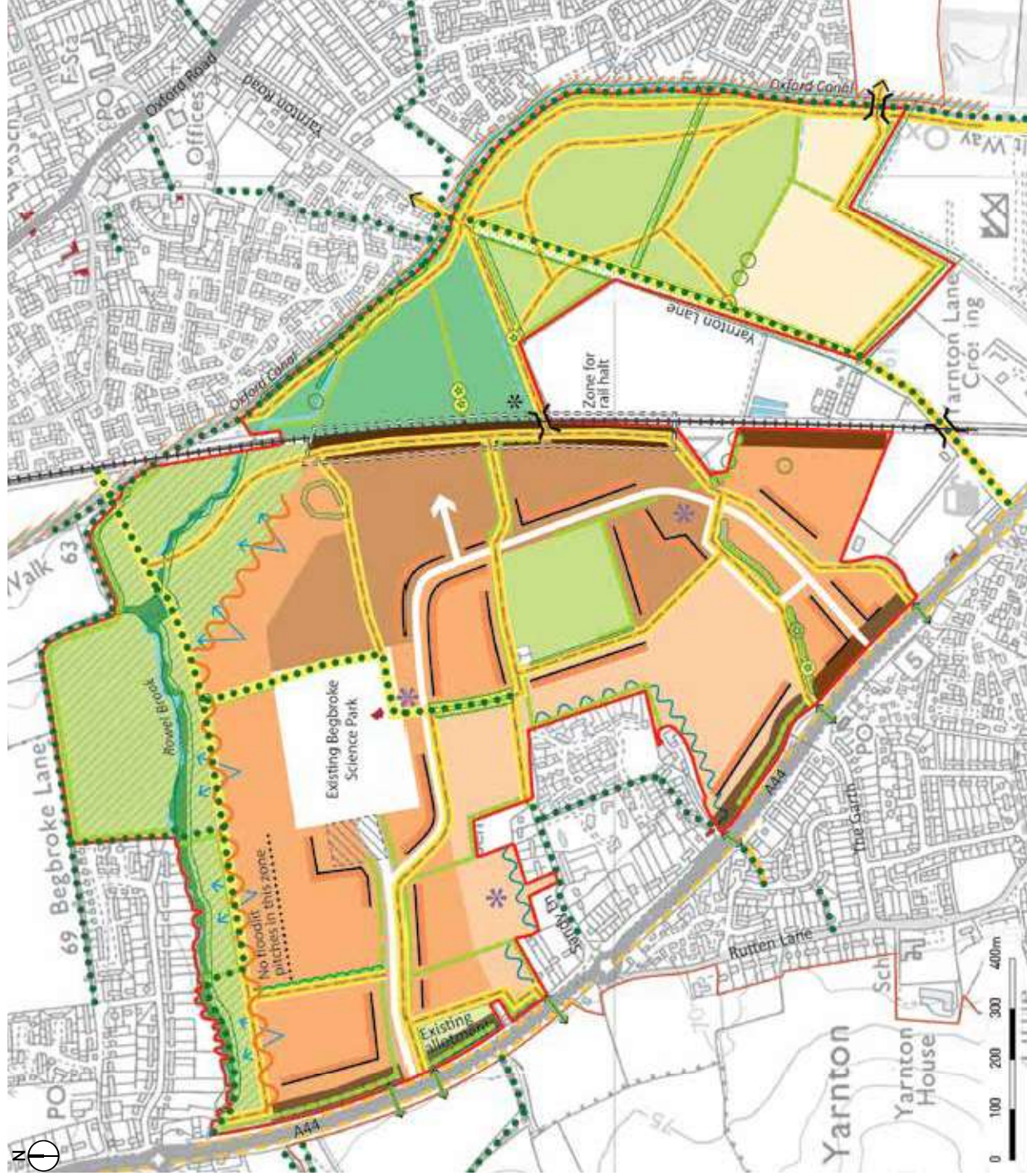


Figure 14: Urban design



### 6.3.1 Science and learning cluster

The emphasis of this character area to the north of Sandy Lane is on supporting science innovation and learning, and this is to be reflected in the innovative and sustainable design of development and the mix of uses. To the north of the primary street the character area will incorporate the secondary school, the expansion of the University's Begbroke Science Park and the provision of residential development overlooking the green corridor. To the south of the primary street a three-form entry primary school is to be provided, together with a local centre, and the potential retention of a hub of small-scale local facilities associated with Yarnton Home and Garden and the allotments.

#### **Development principles:**

To the north and east of the primary street:

- Development is to provide a connected street based layout, integrating residential and employment uses within the street scene.
- Buildings are to be arranged to create a strong frontage to the primary street and the A44, and to integrate with the existing Science Park.
- There is an opportunity for new public squares to the south of Begbroke Hill Farmhouse, and at the western entrance to the Science Park and secondary school.
- High quality, innovative architecture and layouts will be supported which maintain the basic layout principles of good urban design for both residential and employment uses (see Cherwell Design Guide and Local Plan Policy ESD16 and Policy Kidlington 1 of 2015 Plan).
- Employment space development should be compatible with adjacent residential uses and include strong ground floor activity and entrances with the development of a campus style approach to the Science Park linked by pedestrian and cycling networks with landscape and public realm opportunities.
- The layout is to ensure that the location of any larger footprint employment uses, or areas requiring secure boundaries, do not hinder good pedestrian connectivity across the site and with the surrounding foot and cycle network.

- To the north of the primary street, as indicated on Figure 14, building heights of between 2 and 5 storeys are appropriate, subject to assessment of visual and noise impact on existing properties in Begbroke and Yarnton, the Oxford Canal Conservation Area, and other heritage assets, with taller buildings generally located closer to the railway line.
  - Where development abuts the green corridor, it should both front the corridor and create a softened urban/landscape edge which enables views from within the development into the landscape. The potential for green fingers connecting the landscape into the development should also be explored.
  - The layout is to ensure green space provision and connectivity for people and wildlife between and adjacent to the Science Park, local centre and schools noting that the area includes a pond with positive eDNA newt survey results.
  - Homes are to be provided for Oxford's unmet housing needs reflecting the right housing type and size mix to address that need.
  - The secondary school layout and design must be in line with OCC requirements but also designed to provide community access to the sports hall. No floodlit pitches should be located in the northern part of the site adjacent to the Rowel Brook green corridor (see figure 14).
- To the south of the primary street:
- The proposed local centre should provide a frontage to the primary street and proposed public square and relate well to the adjacent primary school.
  - The local centre should have the flexibility to accommodate a mix of uses, with active frontage on the ground floor, and residential or employment uses above.
  - Parking for the local centre should be located in communal parking areas (e.g. a multi-storey provision) and short-term visitor parking and delivery drop off facilities should not dominate but should be designed into a high quality public realm. Surface parking for commercial development will be resisted.

Science and learning cluster: character area location and precedent photos



Location plan



Image credit: Brunwood SciTech

High quality employment space



Three storey townhouses fronting public square



Image credit: Eddington, Cambridge

Mixed use development with a semi-natural landscape setting, Eddington Cambridge



Image credit: Eddington, Cambridge

New local centre market square, Eddington Cambridge

- The existing tree-lined public right of way leading north from Sandy Lane is to be retained providing access to the Science Park, local centre and schools by foot and bike, and terminating in the listed Begbroke Hill Farmhouse and a new public square.
- The school buildings to the north of Sandy Lane will be a maximum of 3 storeys in height, with scale reducing where buildings are to be adjacent to existing residential properties in Yarnton.
- Subject to clear justification, there is an opportunity to retain the existing Yarnton Home and Garden use, which together with proposed residential use could become a mixed-use hub with a new access from the primary street. Any variations on the future proposals for this part of PR8 site will need to be well integrated and complementary to its immediate local context. It is not intended that the Yarnton Home and Garden site will be the main location for local centre uses.

#### Across the character area:

- Where development is proposed adjacent to the A44 and railway line, appropriate levels of noise and air pollution mitigation measures are to be accommodated into the building design. In general, the principles of good acoustic design are to be followed in the site layout and the internal design and specification of properties and gardens to mitigate the impact of potential noise pollution arising from the A44 and railway, while creating an attractive, integrated and accessible development along the frontage to A44.
- The design of the primary school and secondary school is to be in line with OCC requirements (see section 6.6) with access from the primary street and at various locations around the school sites.
- The arrangement of development should respond to the location of the rail crossing (should this be relocated to the north of the existing bridge) to create direct, active travel routes linking Kidlington and the A44 through the site.

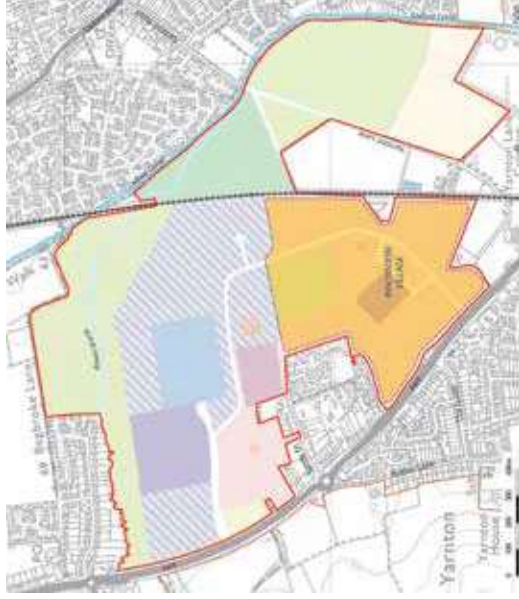
#### 6.3.2 Innovation village

To the south of Sandy Lane, a new village neighbourhood is to be developed focused around a smaller cluster of convenience facilities, a central park and a two form entry primary school. The village is to be integrated with the green corridor and science and learning cluster character areas by the street network, walking and cycling links and hedgerow and drainage corridors. The neighbourhood is to be connected with Yarnton and Begbroke but have a distinctive character and sense of place.

#### **Development principles:**

- Development to the south of Sandy Lane is to create a distinct village character with a connected street-based layout, focused on the convenience facilities, central park and primary school.
- High quality, bespoke and sustainable residential architecture will be supported, including references to vernacular styles and the use of local materials where appropriate (refer to Cherwell Residential Design Guide for materials and vernacular typologies).
- Buildings lining the primary street and bus route, and adjacent to the railway area and convenience facilities are to be a maximum of 4-5 storeys in height, as indicated on Figure 14. Buildings in proximity to existing properties of Yarnton and Begbroke are to be a maximum of 3 storeys, with scale reducing in response to sensitive existing property boundaries and where shading to the school site could occur.
- The primary school is to be located centrally and provide pupil drop off parking, with access from the primary street, and walking and cycling routes connecting with all parts of the development. Should the primary school not be required (with OCC's agreement), this part of the site would be developed for residential.
- A further public square could be created to the west of the proposed rail halt, providing a pedestrian interchange with bus stops on the primary street and access to the proposed pedestrian and cycle bridge over the rail line, there may be an opportunity for small-scale commercial development in this location.

Innovation village: character area location and precedent photos



Location plan



Semi-detached houses and short runs of terraces overlooking the green corridor, Elmbrook Bicester



Local convenience facilities providing active ground floor use, London



A range of property typologies and parking arrangements, Cambridge

- The former landfill site to the south of Sandy Lane is to become a Central Park. It will be connected by existing and enhanced hedgerows along Sandy Lane to the wider green infrastructure network. It is expected that the surrounding built development will front on to it.
- As with the science and learning cluster, where development is proposed adjacent to the A44 and railway line, appropriate noise and air pollution mitigation measures are to be accommodated into the design. In general, the principles of good acoustic design are to be followed in the site layout and the internal design and specification of properties and gardens to mitigate the impact of potential noise pollution arising from the A44 and railway, while creating an attractive and accessible development frontage to the A44.
- Hedgerow and drainage wildlife corridors are to be integrated into the new village neighbourhood in line with overarching green infrastructure development principles.
- The design should provide an appropriate edge to Littlemarsh Playing Fields to the south, and enable a pedestrian and cycling connection to these sports facilities.

### 6.3.3 Canalside green corridor

The third character area covers the northern and eastern part of the site which is designated Green Belt. Uses in this area include public open green space, Local Nature Reserve, Nature Conservation Area and agriculture. It is to be kept free from built development.

Key aims include wildlife protection and improvement, biodiversity net gain, improved access to countryside and nature, health benefits, improved and increased accessibility and connections between communities in the A44 and A4260.

Development principles relating to green infrastructure within this character area are provided in section 6.5.

### Cherwell Local Plan 2011-2031 (adopted 20 July 2015)

Policy ESD 15: The Character of the Built and Historic Environment  
Policy ESD 17: Green infrastructure

### Cherwell Local Plan 2011-2031 – Partial Review (adopted September 2020)

Policy PR8 – Land east of the A44  
Policy PR2 – Housing mix, tenure and size

### Cherwell Residential Design Guide SPD (adopted 16 July 2018)

Chapter 4: Establishing the Structuring Principles  
Chapter 5: Streets and Spaces  
Chapter 6: Building and Plot Arrangements  
Chapter 7: Building Elevations and Details  
Chapter 8: Innovation and Sustainability

### Reference should also be made to:

The Setting of Heritage Assets Historic Environment Good Practice Advice in Planning Note 3 (Second Edition), Historic England 2017  
Increasing Residential Density in Historic Environments, ARUP on behalf of Historic England, 2018

## 6.4 Movement and Access

### 6.4.1 General Principles

The layout of the site is to prioritise movement by active and sustainable modes by creating excellent pedestrian, cycle, wheelchair connectivity within the site, to Yarnton, to allocated sites PR9 and PR7b (via new canal and rail bridges), to Begbroke village and towards Kidlington village centre. In doing so, and by connecting directly with the surrounding street network, the layout will encourage movement by walking and cycling and limit unnecessary car trips.

The design of streets within the site should follow the guidance set out in the Cherwell Residential Design Guide, OCC Street Design Guide and the Manual for Streets, in a manner which is appropriate to the character and quality of place which is to be created as described below. A standardised highways-led layout is not acceptable: carriageway space and turning radii are to be limited (in line with adopted guidance).

### 6.4.2 Vehicle Access

Policy PR8 requires at least two site access points to be provided from the A44 including the use of the existing Science Park access road. The preferred locations are described below.

#### **Development principles:**

- Vehicular access from the A44 in the northern part of the site will be provided from an extension to the existing Science Park access road. The existing junction with the A44 will require reconfiguration with the design determined by the scale of impact of sites PR8 and PR9 assessed together and is to be agreed with OCC Highways. The junction will need to have sufficient capacity to cope with demand from both developments. Bus priority measures on the A44 will likely need to be included together with bus access into the site and pedestrian and cycle crossings on all arms, in line with OCC policy requirements for bus and cycle connections into Oxford and LTN1/20.

- A second access point in the southern part of the site will be provided via a new junction onto the A44 to the north of the existing petrol filling station as indicated indicatively on Figure 15. The design and location of the junction is to be agreed with OCC Highways. It is to incorporate pedestrian and cycle routes and crossings on all arms and accommodate bus movements on the A44 and into and out of the site.
- Sandy Lane level crossing and Yarnton Lane level crossing are to be closed to vehicles. It is assumed that all vehicle access to the site will therefore be from the west. A suitable access to properties to the east of the railway line will be required.
- A potential restricted vehicle access via a bridge further north may be delivered, subject to agreement between Network Rail and OUD. This could provide emergency and refuse vehicle access to properties east of the railway line.
- Crossing opportunities will be explored for alternative modes of transport (e.g. public transport) where this does not compromise design quality or the overall viability of the scheme to deliver affordable housing or other infrastructure, and does not preclude the achievement and timely delivery of Local Plan Partial Review objectives and site allocations as a whole.
- A direct, connecting primary street will be created between the two A44 access points. The primary street will provide access to the Science Park, local centre, primary and secondary schools, potential rail halt and secondary streets providing access to all parts of the site. Cycle and walking infrastructure will be required in line with OCC's standards and LTN 1/20, unless alternative fully segregated facilities are to be provided which offer similar or more direct routes to those facilities.
- The primary street will also form the bus route through the site and so will need to accommodate bus stops, shelters etc.

- The existing vehicular route from A44, Sandy Lane needs to be carefully integrated and managed to prevent new vehicular traffic adversely impacting existing users along Sandy Lane and to avoid rat-running on this route from future development. It is thereby recommended by Oxfordshire County Council that the central part of Sandy Lane be closed to through vehicle movements and only used for access to the primary school, emergency access and other Sandy Lane uses (if required). Sandy Lane, where it merges into the new development will become a high-quality walking and cycling route towards Kidlington.
- Due to a 1 tonne weight restriction on the Sandy Lane canal bridge, with the closure of Yarnton Lane level crossing Network Rail are to provide an alternative access for heavier maintenance and emergency vehicles from the A44 to land and any existing property to the east of the railway line. This may require use of the eastern section of Yarnton Lane.
- Sandy Lane canal bridge to the east of the site is to remain open to provide access for vehicles below the weight limit, towards the potential rail halt.



















### 6.4.3 Pedestrian and Cycle Access

Pedestrian, wheelchair and cycle access points into the site will be provided on all boundaries, connecting north-south and east-west routes across the site with the surrounding area.

#### **Development principles:**

- The following access points for pedestrians, wheelchair users and cyclists are to be provided:
- Access onto the A44 corridor in the northern and southern parts of the site, providing direct access to the bus stops and cycling infrastructure along the A44. These should provide direct connections with A44 pedestrian and cycling crossings and onward routes to the west of the A44 through site PR9 and the existing street network of Yarnton. A new formal pedestrian and cycle crossing is to be provided on the A44 at the existing bus stops in order to provide safe crossing between these and to allow direct connections between PR9 and PR8. The location is to be agreed with OCC Highways subject to detailed assessment and Road Safety Audit.
  - Regular access points from the developable area into public open green space to the north and east, and to the existing network of public rights of way, including connection onto Begbroke Lane and towards Kidlington village centre via the existing canal bridge and level crossing to the north east.
  - Access south towards Oxford City Centre on a new walking and cycling route adjacent to the Oxford Canal or on improved sections of canal towpath.
  - Access to Kidlington from the centre of the site via a new pedestrian/cycle railway bridge and the existing Sandy Lane canal bridge.

6.0 Development Principles

-  Site Boundary
-  Site Boundary of adjoining sites
-  Ditch retained and integrated into site drainage
-  Primary street (bus route)
-  Secondary street access
-  Lanes for access only
-  School site access point\*
-  Vehicular access point (southern location is indicative)
-  Proposed A44 pedestrian/cycle crossing
-  National Cycle Route 5
-  Existing/improved PRow/cycle routes
-  Key new walking/cycling routes
-  Zone for potential rail halt
-  New pedestrian/cycle bridge over railway/canal
-  Potential for alternative bridge further north with restricted vehicular access, subject to agreement between OUD and Network Rail
-  Level crossing closure
-  Bus stop
-  Proposed bus stops (locations to be determined)

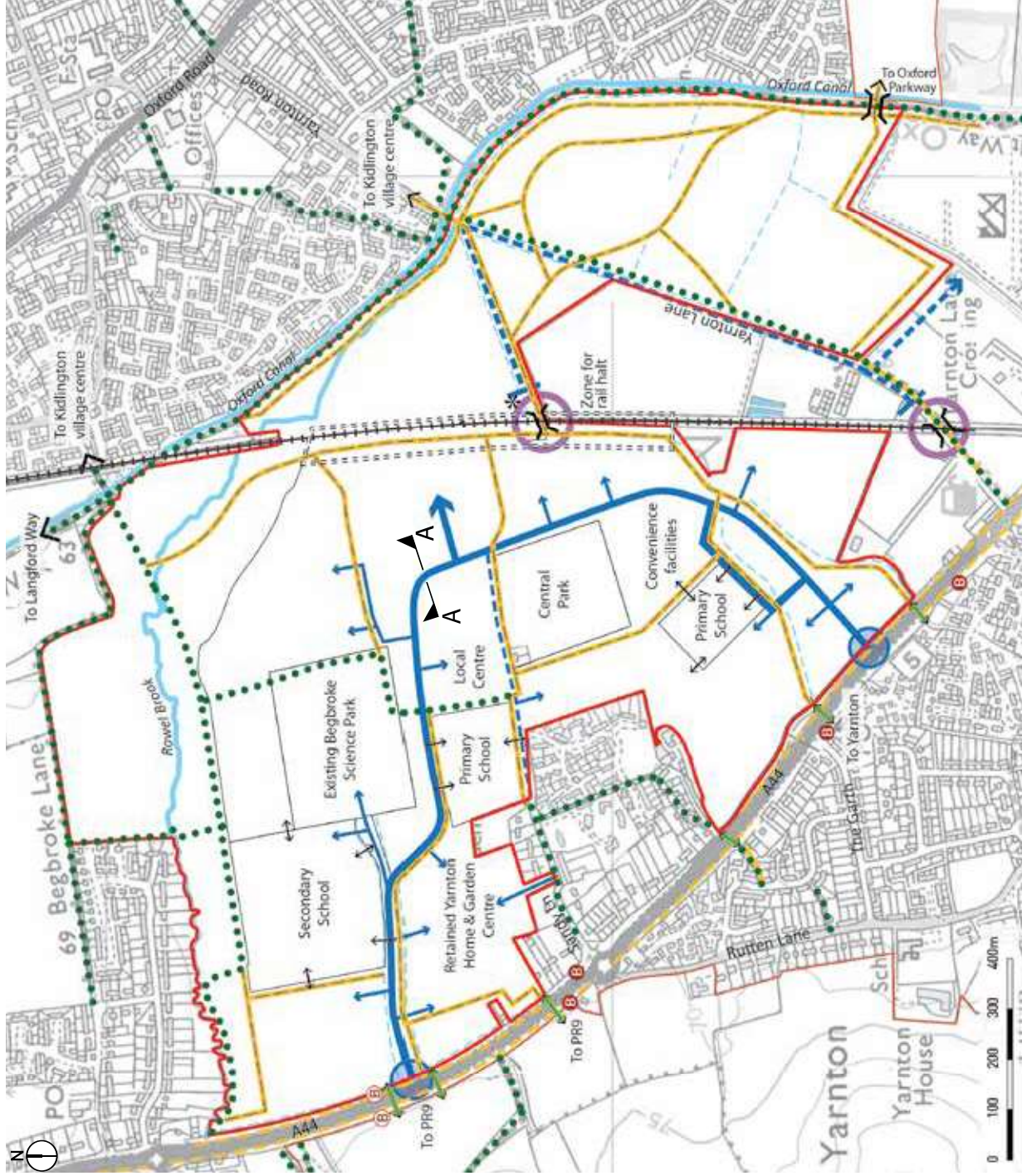


Figure 15: Movement and access

\*School site location subject to further detailed assessment



- Access to PR7b and towards Oxford Parkway via a new pedestrian, wheelchair and cycle bridge over the canal.
- Access into the canal side parkland via the new Yarmton Lane pedestrian and cycle bridge over the railway line.
- Access to Langford Lane/Airport employment via canalside route to the north.
- Potential access to Littlemarsh Playing Fields from the site.

#### 6.4.4 Street Hierarchy and Typologies

The street hierarchy for the site is identified on Figure 15. It follows the street typologies set out in the Cherwell Residential Design Guide SPD. Streets are classified into the following typologies:

- Primary – main streets and high streets
- Secondary – general residential streets
- Tertiary - minor residential streets and lanes (not shown on Figure 15)
- Key walking and cycling routes – green networks/cycleways and pedestrian only routes where commuting activity might be expected or provide a direct route to the science park, local centre or other facilities for non-vehicular users. Development principles for this network is covered under 6.4.5.

All streets across the site should have a maximum design speed of 20mph.

#### Primary Street

The primary street is to provide a through route with bus provision between the two access points onto the A44. It is to give access to the schools, Science Park, local centre and the potential rail halt and the secondary street network serving all parts of the site. The approach must be to ensure that the vehicle priority is below that of cyclists, pedestrians and public transport.

#### Development principles:

- The primary street is to follow the design guidance for main streets and high streets out in chapter 5.0 of the Cherwell Residential Design Guide and the details below.
- The design is to be agreed with OCC and is to have a minimum carriageway width of 6.2m. Swept path analysis will be required to confirm suitability of the route alignment for 12.2m buses. Good practice can also be found in the Stagecoach document Bus Services & New Residential Developments.
- Segregated and formal cycleways of a minimum of 2m in width and footways of at least 2m in width on either side should be included.
- The width of the carriageway is to be balanced by the introduction of street trees and buildings frontages of 3–5 storeys to provide a strong sense of enclosure and appropriate street proportions.
- The street width should vary to accommodate designated on street parking bays with EV charging points and pinch points for traffic calming (which should also be reflected in the building line).
- The overall street network around the school sites is to be designed to ensure that there are no dead ends and the road layout is to allow circular routes.
- Coach drop-off/pick-up laybys (minimum of 18m straight length per coach plus approved entry and exit arrangements such as tapers) are to be provided on the primary street adjacent to the main entrance to the secondary and northern primary school sites. For a 2FE primary school there is to be one coach layby, for a 3FE primary school two coach laybys and for the secondary school 4 coach laybys. N.B. it may be possible for the number of laybys to reduce where they are accessible equally from two schools.

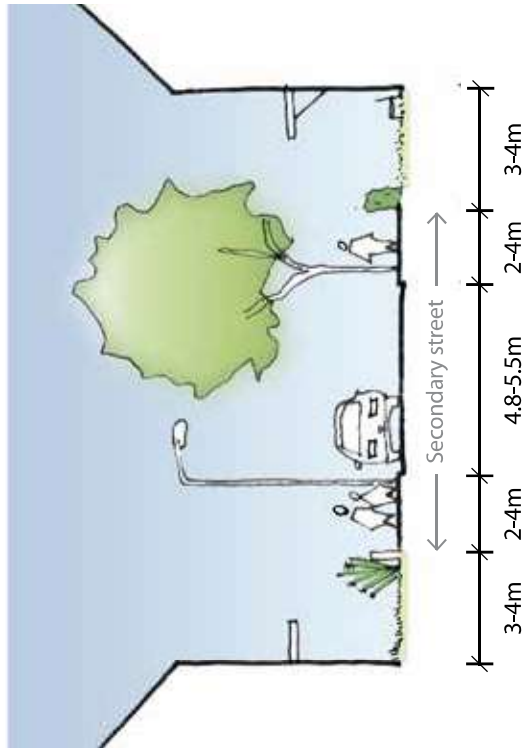
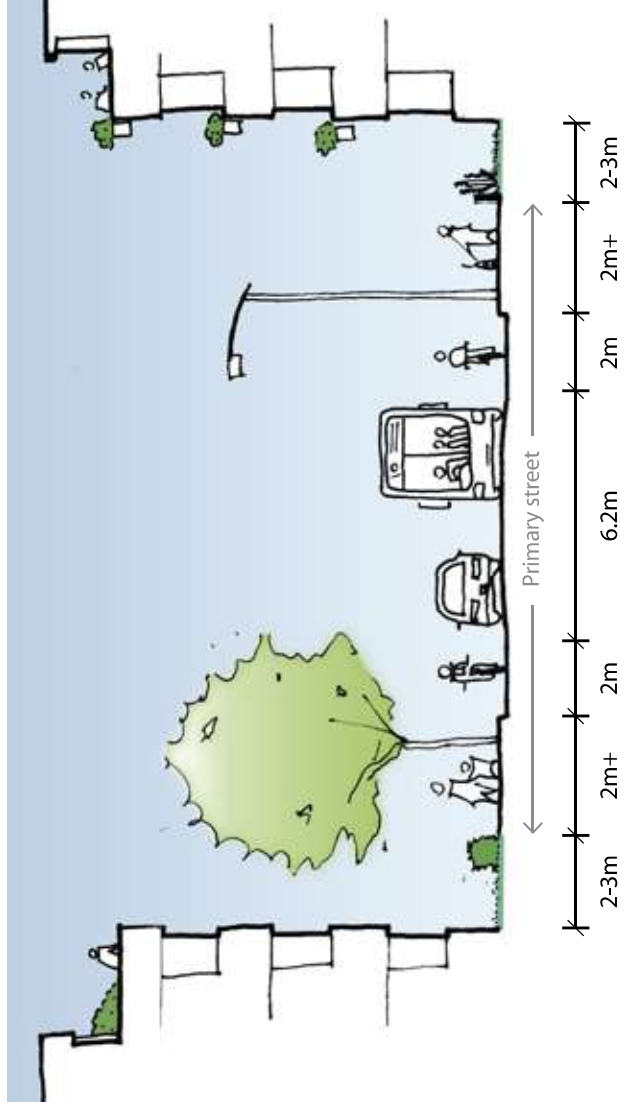


Figure 16: A-A - typical primary street cross section (refer to Figure 15 for section location)

Figure 17: typical secondary street cross section

- The opportunity for Sustainable Drainage Systems (e.g. swales and rainwater gardens) should be considered as part of a connected drainage network
- Street clutter and barriers should be avoided. The inclusion of bollards should be designed to reflect the character of the road. Infrastructure should be designed to be multi-purpose (e.g. include charging points).

#### **Secondary Street**

Secondary streets run off the primary street as a principal connection.

#### **Development principles:**

- The secondary streets are to follow the design guidance for general residential streets set out in chapter 5.0 of the Cherwell Residential Design Guide, and the guidance below.
- Streets should generally accommodate a 4.8-5.5m carriageway.
- Where there are formal cycleways, these should be a minimum 2m in width with footways of at least 2m in width.
- The opportunity for Sustainable Drainage Systems (e.g. swales and rainwater gardens) should be considered as part of a connected drainage network
- Street trees must be included.
- Secondary streets around the school sites are to be designed to ensure that parking and access to the school is well designed and well integrated within the new development.

- Coach drop-off/pick-up laybys (minimum of 18m straight length per coach plus approved entry and exit arrangements such as tapers) are to be provided on any secondary streets adjacent to main school entrances. For a 2FE primary school there is to be one coach layby, for a 3FE primary school two coach laybys.

#### **Tertiary Streets**

Streets that connect to residential areas or where minimal traffic movement from non-domestic use is expected.

#### **Development principles:**

- The tertiary streets are to follow the design guidance for minor residential streets or lanes set out in chapter 5.0 of the Cherwell Residential Design Guide, and the guidance below.
- Streets should generally accommodate a 4.8m carriageway plus footways. On no through routes, or where streets they abut green spaces, the streets may take the form of shared lane of minimum width 6m to allow for a protected pedestrian corridor adjacent to an effective 4.8m road width, subject to the necessary safety audits. On any side where there is no footway, a 800mm maintenance margin is required in addition to this.
- Unadopted, private routes serving multiple properties should be limited, except where specifically agreed with the Council, in line with section 5.2 of the Cherwell Residential Design Guide SPD.
- The opportunity for Sustainable Drainage Systems (e.g. swales and rainwater gardens) should be considered as part of a connected drainage network.
- Street trees, planting and seating must be included.

#### 6.4.5 Walking and Cycling Network

In addition to provision for walking and cycling on the connected street network, a series of new and enhanced walking and cycling links will be provided running north-south and east-west across the site.

A clear understanding of commuter and recreational routes should be developed and form part of the future planning application submission. Commuter routes should be direct and allow for walkers and cyclist to move quickly, not just on highway routes whilst recreational routes should be designed for meandering with seats, landscaping and foraging opportunities.

The movement plan indicates a network of key new walking and cycling routes which connect with existing public rights of way. Important routes include:

- A route running north-south in the centre of the site providing a direct link between Begbroke Science Park, the two primary schools and the local centre. To the north east this route then connects to Kidlington and to the south west it connects to Yarnton following the existing hedgerows and watercourse. This route incorporates sections of existing PRoW which are to be upgraded to enable cycling access where necessary.

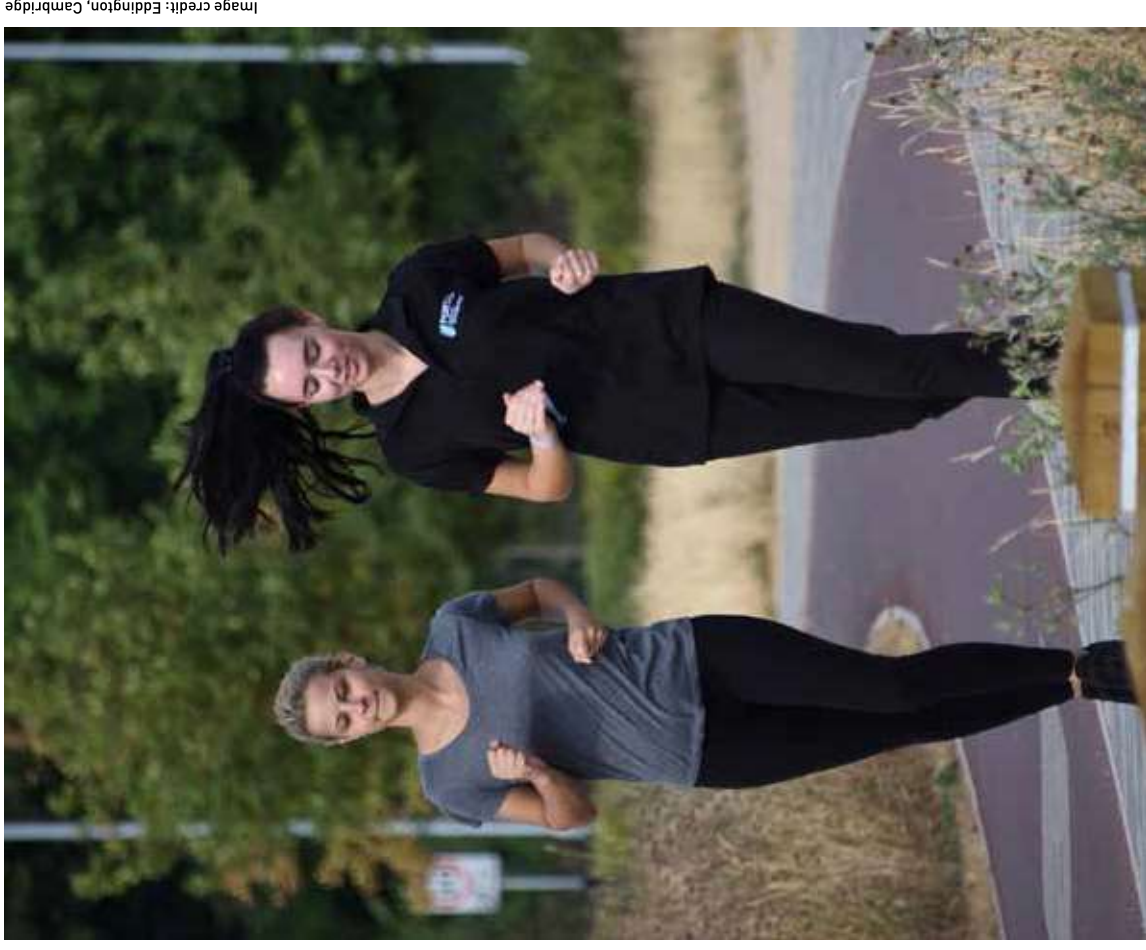
- A route running east from the A44 to the central route (noted above), providing a connection to the secondary school and Science Park.
- Sandy Lane is to become an important cycle, pedestrian and wheelchair route that will provide east-west connectivity between Yarnton, Oxford Canal and Kidlington village centre via a new railway bridge. The new railway bridge will be well-designed and appropriate to the location providing high quality access for pedestrians, cyclists and wheelchair users.
- Yarnton Lane will be enhanced as a pedestrian and cycle link connecting Yarnton and Kidlington via the canal side park and a proposed railway bridge.

- A new public walking and cycling route is to be provided along the Oxford Canal, either through enhancements to the existing towpath or provision of an adjacent new route while retaining the existing canal side hedgerow. This should also extend northwards towards Langford Lane. An elegant new foot, cycle, and wheelchair accessible bridge over the Oxford Canal is to be built, to connect the route with the canal towpath and site PR7b. Land within the PR8 site is to be provided to facilitate this access. The exact location and design of the bridge and towpath improvements is dependent on survey and consultation with the Canal and River Trust and CDC Conservation team and is to be agreed at the outline planning stage. The bridge could incorporate a sculptural quality as part of any public art on the site.
- Improvements to off-site connections south towards Oxford to connect with the Cassington Roundabout to Peartree Interchange scheme will be required and are to be agreed with OCC.

#### **Design principles:**

- The design of pedestrian and cycle routes is to provide a safe and attractive environment for all users. Routes within the developable area should be overlooked by buildings and promote natural surveillance.
- The development is to protect, integrate and extend existing public rights of way and create new routes through the publicly accessible open spaces.
- Routes should connect as directly as possible to public transport access points on the A44, the primary street and the potential rail halt.
- All new pedestrian and cycling routes are to be designed in accordance with Local Transport Note 1/20: Cycle Infrastructure Design published by the Department for Transport. Detailed designs are to be agreed through the pre-application process with OCC and CDC's Development Management Teams, but is assumed that formal cycleways will be a minimum 4m in width providing directional routes in either direction, with separate footpaths of at least 2m in width.

- Street tree planting and seating must be included..
- Lighting is to be provided to walking and cycling routes and PRow within developable area, however this is to be balanced with the impact on wildlife and agreed through the planning application. Refer to MHCLG guidance on light pollution on <https://www.gov.uk/guidance/light-pollution>. This is a particularly important consideration along the Oxford Canal which is currently a dark corridor, is part of a Conservation Area and a Conservation Target Area i.e. no lighting or an exemplary infrared lighting scheme.
- The opportunity for Sustainable Drainage Systems (e.g. swales and rainwater gardens) should be considered as part of these routes as part of a connected drainage network
- Cycle storage solutions should promote cycle usage and be in convenient and visible locations for visitors whilst being secure and safe for longer term storage (e.g. during working day).
- Showers and changing facilities in workplaces should be made available.
- The opportunity to create a Park-run route (either a single 5km route or 2 laps of 2.5km) to promote healthy lifestyle choices and other recreation routes which would support the K5 Better Together Programme should be progressed. This is a progressive scheme with connections and themes to build upon in terms of creating interest and recreational and wellbeing opportunities as well as building character in the landscape function.



Recreational route

Image credit: Eddington, Cambridge



On-street parking



Landscaped, rear parking court which relates well to the properties it serves

#### 6.4.6 Parking

Car parking provision and design will be in line with adopted OCC parking standards and the Cherwell Residential Design Guide SPD Section 5.8 as well as the good practice recommendations in Manual for Streets.

Cycle parking provision is to be in line with OCC's adopted cycle parking standards and must be covered and conveniently located for easy level access to the street. Public cycle parking will be required at the local centres.

#### **Development principles:**

- A range of parking solutions should be used, appropriate to the street and plot typology.
- Car barns and multi-storey parking provision is the preferred solution to parking provision within commercial and higher density residential areas to reduce the need for on-plot parking and to create streets and places for people.
- The Council also advocates the use of unallocated on-street parking wherever possible, to increase flexibility and reduce the number of spaces required overall. This should be integrated into the street design and clearly defined. Special attention should be given to this on the primary street, where parallel bays should be considered to reduce the impact on bus movements.
- Rear courtyard parking is the least preferred solution, but may be necessary for apartment buildings or where a continuous building frontage is required and parking to the front would impact on the quality of the street scene or reduce street enclosure. Rear parking should be clearly related to the properties it serves and provided in small, secure and well overlooked courtyards or private rear gardens.
- Electric charging points should be provided in line with national and local standards either on plot or serving on street parking bays. If on street, the design should consider innovative solutions to limit visual impact e.g. pop-up charging points. All spaces designed for disabled parking provision should include EV charging points to avoid discrimination.

- Public cycle parking is to be provided adjacent to children's play spaces, the local centre, schools, the Science Park and close to bus stops.

#### 6.4.7 Emergency Access and Refuse Collection

Streets within the development will be designed to allow appropriate access for emergency and refuse vehicles. Developers are advised to refer to Cherwell Residential Design Guide for the requirements for service access and refuse bin storage design and collection points.

#### 6.4.8 Public Transport

As noted above, the primary street is to accommodate a bus route serving the development. Stops should be located close to schools, the Science Park, local centre, residential areas and the proposed rail halt and should enable all parts of the development to be within a short walking distance of a stop, including those located on the A44. Provision should be made for at least four pairs of bus stops on the primary street with a further additional pair on the A44 close to the northern site access. As a minimum, bus stops will require a shelter, pole, flag and timetable case, marked clearway and real time passenger information in line with OCC specifications.

CDC / OCC support the potential provision of a rail halt in the zone shown on Figure 12 and 0.5 ha of land is to be reserved to accommodate this. Further evidence of need is required to support the provision of the halt and assessment of the best location including consideration of access, land ownership, impact on the proposed nature conservation area and relationship to other uses is to be provided. Should a halt be developed, it is assumed that this will have one platform on each side of the tracks measuring approximately 150m in length and 6m in width. No buildings will be required to serve the rail halt due to the limited frequency of services, however shelter, ticket machine, lighting and tannoy will be required.

Limited vehicle access for drop-offs will be required and secure cycling facilities are to be provided in a manner to encourage cycling to and from the station.

Car parking will be limited to disabled parking bays in order to prioritise walking and cycling to the rail halt. The design of the halt is to be agreed with Network Rail and coordinated with the design of the railway bridge and east-west walking and cycling route. The rail halt shall be served by a direct pedestrian/cycle route connecting the chosen location with the local centre, nearest bus stops and an east/west route to Yarnton and Kidlington.

#### **Cherwell Local Plan 2011-2031 (adopted 20 July 2015)**

- Policy SLE 4: Improved Transport and connections
- Policy ESD 1: Mitigating and Adapting to Climate Change
- Policy ESD 15: The Character of the Built and Historic Environment
- Policy ESD 16: The Oxford Canal

#### **Cherwell Local Plan 2011-2031 – Partial Review (Adopted September 2020)**

- Policy PR4a: Sustainable Transport
- Policy PR8 – Land east of the A44

#### **Cherwell Residential Design Guide SPD (adopted 16 July 2018)**

- Chapter 4: Establishing the Structuring Principles
- Chapter 5: Streets and Spaces
- Chapter 8: Innovation and Sustainability

#### **Oxfordshire County Council:**

- Street Design Guide
- Cycling Design Standards
- Walking Design Standards
- Parking Standards for New Developments

#### **Reference should also be made to:**

- Bus services & New Residential Developments – Stagecoach, 2017
- Buses in Urban Developments – CIHT, 2018

## 6.5 Green Infrastructure

### 6.5.1 Landscape character areas

In line with Policy PR8, significant areas of open landscape are to be retained in the eastern part of the site adjacent to Rowel Brook and the Oxford Canal. These are to be connected into the development via existing hedgerows, watercourses and new greened walking and cycling routes to create a multi-functional green and blue infrastructure network across the whole development site providing a range of ecosystem services.

Policy PR8 requires a Biodiversity Impact Assessment be submitted as part of the planning application for the site and a supporting Biodiversity Improvement and Management Plan. The Environment Act 2021 makes biodiversity net gain mandatory for all but small sites and some exemptions from an as-yet unconfirmed date in November 2023 and for small sites from April 2024. In recognition of that, in October 2019, the Council's Executive endorsed seeking a minimum of 10% biodiversity net gain through engagement with the planning process. PR8 Policy delivery requirements 8, 9, 10, 11, 18, 19, 20, 21, 29 and 37 indicate measures to be incorporated into the development scheme and are reflected below.

#### **Development principles:**

##### **Canalside Green Corridor character area**

The canalside green corridor is to incorporate the following green infrastructure typologies including (from the south to the north):

- Retained agricultural land, which is to incorporate new perimeter walking and cycling routes connecting with Yarnton Lane, the Oxford Canal towpath and the proposed new bridge over the canal towards site PR7b.
- Informal public parkland adjacent to the canal and opposite Kidlington football club serving residents of Kidlington, PR8 and PR7b. This new parkland is to be publicly accessible and have an emphasis on nature conservation, informal recreation and natural play with a network of new footpaths and cycleways. There is an opportunity to provide a small visitor

facility, play space or information point serving both the parkland and Oxford Canal. Yarnton Lane which crosses the parkland may accommodate vehicle movements, in which case safe pedestrian crossing points are to be provided.



























- Nature Conservation Area to the north of Sandy Lane and east of the railway line which is intended to have no public access but fenced perimeter pedestrian and cycling routes adjacent to the canal and along Sandy Lane. The perimeter fence should allow animal ingress and be surrounded by planting to minimise the visual impact. The nature conservation area is to be designed to support ground nesting birds and mitigate impact on the SSSI to the north as a result of the development.
- A publicly accessible Local Nature Reserve adjacent to the Rowel Brook in the northern part of the site which is to be designed to enhance the existing footpath network along the Brook and extend and connect existing habitats in this area. The SSSI adjacent to public rights of way is to be fenced and appropriate design measures are to be taken to prevent access around ponds/water vole habitat.

#### **General principles**

- A long term management and maintenance strategy for habitat and green infrastructure is required to ensure all these elements are well designed, delivered (and eventually managed) to a high standard; and to ensure that residents have lasting access to nearby high quality green space and natural areas.
- Other features such as information boards, seating and infrastructure associated with walking and cycling should be informed by the landscape. Lighting should also be low level and designed with ecological enhancement as the basis and clearly understood.
- Interaction with the canal corridor should allow for movement from the canal towpath at appropriate points but should retain and enhance landscaping and the character of the Conservation Area and the listed structures in this context.



6.0 Development Principles

-  Site Boundary
-  Site Boundary of adjoining sites
-  New local public green space/corridors (indicative location)
-  New central park
-  Informal public parkland
-  Local Nature Reserve
-  Nature Conservation Area (limited public access)
-  Revised Green Belt
-  Retained Agricultural Land
-  SSSI
-  NERC ACT. 541
-  Priority Habitat Inventory
-  Broad-leaved semi-natural woodland
-  Retained hedgerows
-  Reinstated hedgerow
-  Tree considered to be of Veteran status
-  Tree considered to be of Transitional Veteran status
-  Other retained trees
-  Retained group of trees
-  Continuation of tree-lined avenue
-  Ditch retained and integrated into site drainage
-  Equipped play area (indicative location)
-  Parkland/visitor hub/play space
-  Indicative locations for allotments
-  Landscape design of noise attenuation
-  Soft edge between urban and landscape
-  Proposed A44 pedestrian/cycle crossing
-  National Cycle Route 5
-  Existing/improved PRow/cycle routes
-  Key new walking/cycling routes
-  Zone for potential rail halt
-  New pedestrian/cycle bridge over railway/canal

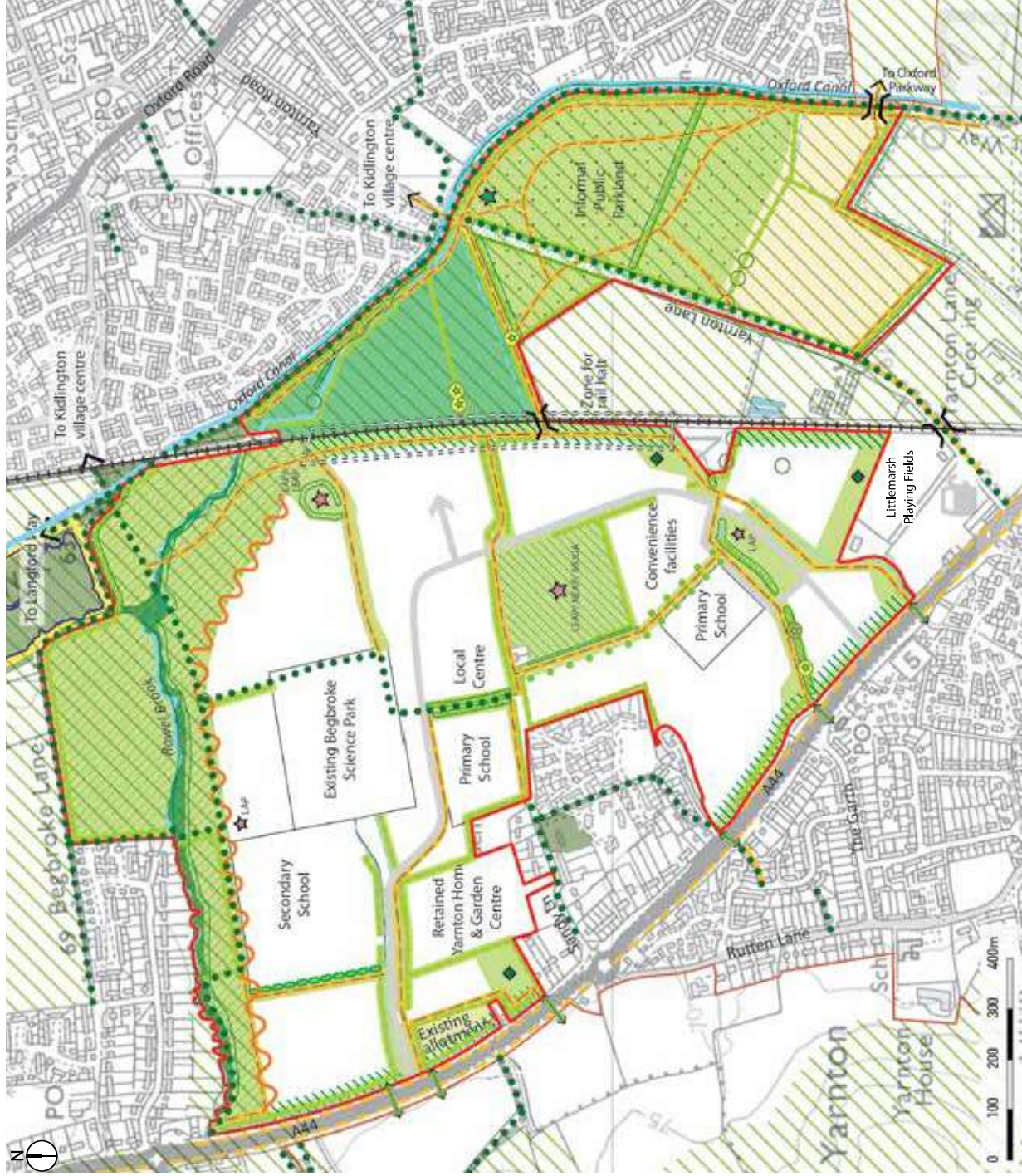


Figure 18: Green infrastructure

Canalside green corridor: character area location and precedent photos



Location plan



Green infrastructure precedents



### 6.5.2 Other green infrastructure features

#### Central Park

Through the landowner's masterplanning work, the former landfill site has been identified as an opportunity for a central park and formal space. The design and detail has not been formulated but is welcomed. Given the size and space available it would be beneficial to consider best practice and examples from around the UK and abroad to create a space which builds on its scale but also feels intimate, safe and usable.

The space presents an opportunity for features such as table tennis tables, adult gyms, a skate park, children and young people's play and meeting space, and a formal landscape for public art and exhibitions as well as information boards for events and community organisations.

There is an opportunity for commercial cafés or restaurants on or overlooking the space. The space should also include significant planting and shade as well as other areas for relaxation including aromatic and sensory gardens.

#### Community Gardens and Orchards

Areas within green infrastructure which can present a formal area for seating and reflection and also to promote edible landscapes and foraging. These spaces should be beneficial to both users and wildlife and should include the opportunity for shade as well as areas for relaxation including aromatic and sensory gardens.

#### Allotments

Community allotments totalling 1.8 hectares in size are to be incorporated within the allocated developable area. This is in addition to the retention or re-provision of the existing allotments fronting the A44. The design, location and character of the allotments are to be agreed with the Council and to be delivered in line with adopted standards. Proposals to relocate the existing allotments will require justification and should minimise the impact on existing users and residents of Begbroke and Yarnton.

Reference should be made to the principles of 21st Century Allotments in New Developments by the National Allotment Society.

*"A well-designed allotment site makes for a pleasant and sociable inclusion in a new development, without any need for screening/obscuring or seclusion on the periphery of the development site. Generally, the public do value allotment sites and their garden aesthetic as part and parcel of the character of a neighbourhood, along with the associated contribution to the natural world and interconnected networks of Green Space Infrastructure.*

*However, allotments designed into the outer spatial periphery of a new development can be vulnerable targets for intruders, vandalism and crime. This can affect social inclusion, as vulnerable users can be deterred from taking up plots and existing plot holders can be driven to quit. A site which is miles from the nearest houses and which can only be reached by car will increase carbon emissions and deter potential users without access to a vehicle."*

*By applying the spatial design principles of 'The 20 Minute Neighbourhood' and integrating allotments into the heart of the new development's built design, social inclusion and durability can be designed-in to ensure ongoing access for all and without increasing car use."*

#### Other features

- Local pocket parks and green spaces are to be created within the developed area across the site to provide space for informal recreation and relaxation, drainage features, play and to soften the urban character of the development. There is an opportunity for such a park to the south of the local centre, where green space provides a buffer to existing hedgerow and watercourse and could provide a play space close to the primary school.
- Existing individual and groups of veteran, transitional veteran, high and moderate quality trees are to be retained. Appropriate buffer zones are to be provided to avoid root damage and should be considered when planning sustainable drainage infrastructure.

- Existing intact species rich, and other hedgerows within the site will be retained as far as possible. When the need to cross them occurs, existing gaps will be used wherever possible. A grassland habitat buffer of minimum 5m is to be introduced on either side of the hedgerows, subject to hedgerow root protection area in compliance with BS5837.
- Individual native trees will be planted to form an avenue giving enclosure to the primary street, along the buffers to hedgerows, within public open spaces, site boundaries, as street trees on secondary streets and within private gardens. The overshadowing effect on gardens and windows from proposed trees should be minimised by planting small/medium native trees (i.e. Field Maple). Reference should be made to The Trees and Action Design Group's guidance 'Trees in the Townscape: A Guide for Decision Makers', 2021. Design of street trees requires collaboration of engineers, arboriculturalists and landscape architects in the earliest stages of the design process to achieve the desired effect.
- Street tree species and details of root protection and canopies in relation to adopted carriageways are to be agreed with OCC.
- Where front gardens or privacy strips are provided these are to be planted to maximise biodiversity gain. Tree and shrub planting should be incorporated into the design of the play area and any rear lanes and parking areas. For the health of the children, tree and shrub planting associated with play areas must not be spiny or thorny and be non-toxic.
- The scheme is to include provision of in-built bird and bat boxes, wildlife connectivity between gardens and the provision of designated green walls and roofs where viable. Refer to the Council's Biodiversity and the Built Environment report (2009) for recommendations on establishing wildlife habitat in buildings.



Community gardens and food growing

There should be no incongruity in the design layout of housing plots and public open space: the boundaries must not be blurred (plot boundaries should be defined by walls, fences and formal hedges or other clearly defined boundary treatment). Existing features such as retained hedgerows should have enough usable open space between the plot line and the hedgerow, with an intervening path or road to clearly define boundary ownership and minimise the possibility of illegal land encroachment in the future.

### 6.5.3 Play Space

In agreement with CDC, different types of equipped play areas are to be provided within the site in safe, accessible locations. Potential locations of public play spaces are shown on Figure 18, but alternative locations would be considered.

#### **Development principles:**

Play spaces are to be provided within the PR8 site in line with the following design requirements:

- **Two Local Area for Play (LAP)** for 2 to 6-year old children potentially located within small pocket parks within development blocks:
  - Minimum 100 sq. m (10m x 10m) equipped activity zone set within a landscaped area designed to provide a safe area for alternative play for children aged 2 to 6. The size of the landscaped area (incorporating the equipped activity zone) will be informed by the development context (acknowledging activity zone buffer requirements) and local design guidance.
  - A minimum of 3 individual items of play equipment of an urban character suitable for a range of play experiences and/or single multi-functional play units.
  - The equipped activity zone should be located a minimum of 5m from the nearest dwelling boundary. The landscaped area around the equipped activity zone could be used to incorporate this buffer.
  - Seating, landscaping to provide shade and appropriate lighting should be provided. Litter bins and dog bins would also be appropriate.

- **One combined LAP and Local Equipped Area for Play (LEAP) to be** provided for 2 to 8-year old children, potentially located in the northern part of the site:
  - Minimum 500 sq. m equipped activity zone set within a landscaped area designed to provide a safe area for alternative play for children aged 2 to 8. The size of the equipped activity zone should be a minimum of 10m x 10m in respect of the LAP element and 20m x 20m in respect of the LEAP element. The size of the landscaped area (incorporating the equipped activity zone) will be informed by the development context (acknowledging activity zone buffer requirements) and local design guidance.

- A minimum of 8 individual items of play equipment for a range of different play experiences and/or a number of multi-functional play units, depending on the design layout of the play space.
- The equipped activity zone within the landscaped area should be located a minimum of 10 m from the nearest dwelling boundary and 20m from the nearest habitable room façade. The landscaped area around the equipped activity zone could be used to incorporate this buffer.
- **One combined LEAP, Neighbourhood Equipped Area for Play (NEAP) and Multi-use Games Area (MUGA)** for 4 to 16-year-old children, potentially located in the new central park:
  - Minimum 2400 sq. m equipped activity zone comprising an area of play equipment and structures, and a hard-surfaced area of at least 465 sq. m, set within a landscaped area designed to provide a safe area for alternative play for children aged 4 to 16. The size of the equipped activity zone should be a minimum of 20m x 20m in respect of the LEAP element, 31.6m x 31.6m in respect of the NEAP element and 40m x 25m in respect of the MUGA element.

- The size of the landscaped area (incorporating the equipped activity zone) will be informed by the development context (acknowledging activity zone buffer requirements) and local design guidance.
- A minimum of 13 individual items of play equipment for a range of different play experiences and/or single multi-functional play units. The design should incorporate both urban and natural elements.
- The equipped activity zone within the landscaped area should be located a minimum of 10 m from the nearest dwelling boundary and 20m from the nearest habitable room façade in respect of the LEAP element and a minimum of 30m from the nearest dwelling boundary in respect of the NEAP and MUGA elements. The landscaped area around the equipped activity zone could be used to incorporate this buffer.
- Play areas are to be well overlooked. They should be located within the 400m walking distance of all new homes within the development and close to pedestrian and cycling routes.
- In respect of Health and Safety, public play space and play equipment are to be designed to the most current safest, standards possible, to minimise the risks for children. Refer to Play Safety Forum: Managing Risk in Play and RoSPA.
- The location and design of play areas is to consider the risks to children's safety in relation to any areas of water including features forming part of the SuDS system (see 6.5).
- All play surfaces, gate openings are to be accessible for disabled children, parents and carers with limited mobility. Each public play space should accommodate play equipment specifically designed for disabled children.
- Play areas are to be constructed from robust and durable materials to last into the future. Full construction details are required for planning approval under reserved matters. Valid suppliers' guarantees for play equipment, furniture and safer surfaces should be provided.



- There are to be no underground or above ground utilities within play areas given the potential disruption to children's physical and social development when a play area has to be closed for essential maintenance and refurbishment of such utilities.
- The public play space locations are not to be used for constructor's compounds, contractor parking, or storage of building materials. This is to prevent the contamination and compaction of topsoil and subsoil, resulting in a health risk for children.

#### 6.5.4 Blue Infrastructure

In addition to Rowel Brook, which runs in the northern part of the site, there are a number of minor existing watercourses, ditches and surface water flow routes located on the site which are to be retained and incorporated into overall drainage strategy.

#### **Development principles:**

- A minimum of 3m landscape buffer is to be provided on either side of the existing watercourses and ditches. The buffer should be publicly accessible open space to ensure continued maintenance and access.
- Existing and retained drainage features are to be designed as an integral element of public open spaces and streets, creating environments for informal recreation and habitat creation.
- Sustainable Drainage Systems (SuDS) within the development site will be carefully designed in line with the principles provided in CIRCA SuDS Manual (C753), the Chervell Residential Design Guide section 4.7 and the Local Standards and Guidance for Surface Water Drainage on Major Development in Oxfordshire (2018). Open drainage systems including ponds and swales should be used wherever possible, rather than crates, with SuDS features incorporated throughout the site, rather than relying on strategic ponds. The location for drainage features is to be assessed in a site wide drainage strategy and to be agreed in detail with Oxfordshire County Council Drainage Team.

- On-surface stormwater storage on school sites can present significant health and safety and management risks and attenuation/tank storage is no longer acceptable due to long term sustainability. Therefore, on-surface attenuation provision that account for the outfalls from school sites shall be provided external to any school sites. This on-surface water storage shall form part of the overall surface water management infrastructure and shall fall under the responsibility of the appointed Management and Maintenance Company to maintain in perpetuity. This will increase sustainability and maximise environment gain through water resources, biodiversity, landscape, education functionality and amenity, as well as reduce overall capita and maintenance liability.
- Groundworks associated with drainage must avoid damage to existing trees and hedgerows and their root protection zones.
- Any water to be discharged into the canal as part of the site drainage strategy will require agreement from the Canal and River Trust.

#### 6.5.5 Definition and Treatment of Green Belt Boundary

The site will be developed in a way that respects its edge of Green Belt location and does not harm the Green Belt's visual amenities.

The new Green Belt boundary will be clearly defined within the site by the railway line, and the edge of the built-up area adjacent to the Rowel Brook local nature reserve.

**Cherwell Local Plan 2011-2031 (adopted 20 July 2015)**

- Policy BSC 10: Open Space, Outdoor Sport and Recreation Provision
- Policy BSC 11: Local Standards of Provision- Outdoor Recreation
- Policy ESD 3: Sustainable Construction
- Policy ESD 5: Renewable Energy
- Policy ESD 6: Sustainable Flood Risk Management
- Policy ESD 7: Sustainable Drainage Systems (SuDS)
- Policy ESD 10: Protection and Enhancement of Biodiversity and the Natural Environment
- Policy ESD 11: Conservation Target Areas
- Policy ESD 13: Local Landscape Protection and Enhancement
- Policy ESD 14: Oxford Green Belt
- Policy ESD 15: The Character of the Built and Historic Environment
- Policy ESD 17: Green Infrastructure

**Cherwell Local Plan 2011-2031 – Partial Review (adopted**

**September 2020)**

- Policy PR3: The Oxford Green Belt
- Policy PR5: Green Infrastructure
- Policy PR8 – Land east of the A44

**Cherwell Residential Design Guide SPD (adopted 16 July 2018)**

- Chapter 4: Establishing the Structuring Principles
- Chapter 5: Streets and Spaces
- Chapter 8: Innovation and Sustainability



Landscaped attenuation basin



## 6.6 Community infrastructure

In line with Policy PR8 the development is to provide a local centre on 1ha of land and three schools:

- a two form entry primary school on 2.2ha of land
- a three form entry primary school on 3.2 ha of land
- a secondary school on 8.2 ha of land incorporating a 4 court sports hall

The exact size of the required school sites is to be agreed with OCC with consideration of the site constraints and topography and the timing will need to be agreed as part of the development delivery.

Policy PR8 requires shared community use of the school sports hall. Through the development brief process, it has been agreed that a community access agreement could alternatively be provided by a financial contribution based upon CDC adopted SPD standards to support the delivery of formal indoor sport on the site or in an appropriate location if use of the school sports hall was not appropriate for management reasons. However, such access is commonplace nationally and exceptional circumstances would need to be demonstrated as to why the development cannot be co-located and community access cannot be secured.

In addition, the size of the development is large enough to justify a standalone community hall on site. Ideally this would be located within (or close to) the local centre.

### 6.6.1 Schools

The location of the schools as shown on the PR8 policy map has been reviewed by OCC during the development brief process and a revised indicative location is presented in Figure 12. The size, shape and location of the proposed school sites is indicative and will be subject to further detailed assessment as part of the planning application process, and agreement through S106, and should reflect land equalisation terms/values as agreed between the promoters for ensuring successful and timely delivery of the facilities on site.

To ensure that the school sites are in the optimal location and layout for satisfactory education provision, the design shall comply with the following OCC design requirements, processes, interrogations and checklists as described within the following OCC documents:

- Information and process required to assess the suitability of a school site
- Design criteria for Primary school sites or Design criteria for Secondary school sites
- Education checklist
- Draft drop-off standards at new primary schools

The need for two primary schools on PR8 is maintained in line with Policy PR8, until such time as there is the evidence that one new primary school on PR8 (the three form entry) plus the expansion of William Fletcher Primary School on site PR9 would suffice to meet population needs and that all the necessary approvals are in place for the expansion of William Fletcher Primary School. If it is shown that only one school is required the land released would be suitable for residential development.

#### **Design principles:**

- The layout and size of the school sites is to be guided by OCC with school classrooms facing due north and south in line with OCC guidance, and accommodating outdoor learning space. Any relocation or change in shape and/or dimensions of the school sites will require a proving layout, undertaken by OCC, to verify the change.
- The schools are to be located in a less steep part of the site. It is likely that some adjustment of levels will be required to meet maximum gradients for vehicular and pedestrian accesses of 1:21 from the adopted highway to the school boundary and appropriate internal site levels. Where level adjustment needs to take place, it shall take place outside of the school site and where that is not possible additional area shall be added to the school site. Level changes within the site will only be accepted where they do not create a

‘ditch’ along the boundary or generate sunken or raised access routes across the school site. All levels shall facilitate level access across the site that shall not discriminate against any pupil such that they are able to take the same route, whatever their disability, as their friends.

- All levels shall facilitate level access across the site that shall not discriminate against any pupil such that they are able to take the same route, whatever their disability, as their friends.
- The school playing fields area, based on BB103, shall have the appropriate gradients, no steeper than 1:100 along the line of play and 1:50 across. N.B. the line of play shall be approximately north/south in line with best practice. The remaining site area shall be a level plateau.
- The maximum noise level allowed for the school site is 50dB LAeq at the school boundary. When locating the school sites, it will need to be demonstrated that the noise levels, once housing and other acoustic mitigation factors are taken into account, meet the required noise level standard.
- Vehicle access including coach access, and parking is to be provided in line with OCC guidance (indicative access points are shown on Figure 15).
- The school sites are to have easy access from east-west and north south walking and cycling routes including Sandy Lane and the north-south route.
- The boundaries of the school sites are to be designed in keeping with the surrounding public realm of the character area in which they are located.
- The northern primary school is to be designed as an element of the local centre.
- Pupil drop off parking should be provided within the local centre as although walking and cycling are preferred it is recognised that, for a variety of reasons, for some parents driving to or from school may be the only option.
- Adjacent building shall be of a height and location such that they do not shade the school sites.

### 6.6.2 Local centre

The local centre is to be positioned in an accessible location, linked to other facilities such as the schools. Policy PR8 identifies a preferred location for the local centre in the centre of the southern part of the site occupying around 1ha of land, on the basis that the proposed local centre will need to provide for the neighbouring villages as well as the future occupiers of all parts of the development.

Through the development brief process, it has been proposed that local centre is located further north, to the south of the Science Park and adjacent to the 3 form entry primary school.

A smaller cluster of convenience facilities will be provided in the southern, central part of the site. The existing Yarnton Home and Garden centre may be retained providing further small scale facilities in the north of the site, particularly within the early phases of development.

#### **Development principles:**

- The local centre will form a social focus for the neighbourhood providing a mix of small scale community, business and retail uses. As set out in Policy PR8 the Local Centre shall include provision for:
  - local convenience retailing (use class E - no more than 500 square metres net floorspace and no less than 350 square metres)
  - ancillary business development (use class E) and/or financial and professional uses (use class E);
  - a café or restaurant (use class E);
  - the provision of a community building to required standards providing the opportunity for social and childcare facilities,
  - the opportunity for required health facilities
  - Pupil drop off parking

- The local centre is to be highly accessible, located close to the primary street adjacent to a bus stop, and at the junction of north south and east-west pedestrian and cycling routes. Development is to be of the highest quality with the opportunity to provide a landmark/nodal development.
- It is to be designed in conjunction with the primary school and proposed public square and well connected to the central park to the south, the Science Park and listed building to the north, to create a cohesive focal point for the development.
- Buildings are to be set within high quality public realm, fronting a public square supporting spill out activities and community events. The relationship of the square and local centre to the primary street requires careful consideration to create a pedestrian focused space.
- Discussions should occur, for example on a library drop off facility and/or medical outreach location (i.e. doctor or dentist) and location for social services functions in the community. Meeting rooms for parish councils and residents groups and flexible work hubs should be considered.
- The Local Centre should also be the information and sales centre for the wider development, lessening the need for temporary show homes and other such activity which generate a carbon footprint.

#### **Meeting places in the Local Centre**

The Local Centre should consider both indoor (e.g. a winter garden) and outdoor meeting places and space that is capable of being used and attractive throughout the year and irrespective of the weather. The centre could also include event/meeting space for seasonable markets, a community Christmas tree and remembrance memorial, for example.

Play space should be incorporated into the public spaces of the local centre itself and should be non-specific but cater for a range of ages (including, for example, adult gyms and youth activity such as a landscape designed for skateboard interest and being durable to accommodate such activity)

E-bike and E-scooter hire and charging locations together with public art and science park displays should also be included.



#### **Cherwell Local Plan 2011-2031 (adopted 20 July 2015)**

Policy BSC 7: Meeting Education Needs

#### **Cherwell Local Plan 2011-2031- Partial Review (adopted 7 September 2020)**

Policy PR5: Green Infrastructure

Policy PR8 – Land east of the A44

#### **Reference should also be made to:**

Oxfordshire County Council design criteria for schools:

- Information and process required to assess the suitability of a school site
- Design criteria for Primary school sites
- Education site checklist

### 6.7 Heritage and archaeology

The site is located to the south east of Begbroke Conservation Area and to the west of the Oxford Canal Conservation Area. The principal built heritage assets are the former Jacobean farmhouse, Begbroke Hill Farmhouse and its associated walled garden which is Grade II listed and form part of Begbroke Science Park, and the listed bridges to the canal (Bridges 227 Buller's Bridge and 228 Yarnton Lane Bridge). There are a number of other heritage assets in the surroundings of the site (refer to section 4.0). The site is located in an area of considerable archaeological interest with a number of sites recorded with cropmarks including later prehistoric settlement sites and Bronze Age barrows.

Planning applications for development on the site will need to include an archaeological desk-based assessment and appropriate mitigation strategy. A programme of archaeological evaluation ahead of the determination of planning application will be required.

#### **Development principles:**

- The canal-side green corridor and eastern development parcels are to be designed sensitively in response to the setting of the Oxford Canal Conservation Area and consideration of potential visual impact.
- The Jacobean farmhouse and the walled garden are significant features which are well formed and would need to be understood fully in the development of the masterplan. The opportunity to create meeting places which utilise the character of the existing farmhouse would be an opportunity. The current setting is formed to the south by low walls and limited built infrastructure alongside mature planting. This would need careful attention with limited new infrastructure introduced in this location.

- In particular, the design of the primary access road, local centre and primary school to the south of Begbroke Science Park are to be designed sensitively in response to the setting of the farmhouse including retention and extension of the existing tree-lined cycling and walking route leading to the farmhouse and the creation of a high quality public square.
- The design in the southern part of the site is to be sensitive to the setting of the Grade II listed Tudor Cottage outside the site.
- Any proposals for the Jacobean farmhouse are likely to need full planning and listed building consent but an outline planning permission should set out broad feasibility and principles of how the farmhouse will be integrated into the development and define and consider the setting of other listed buildings and structures which will be impacted upon by the development.

#### **Cherwell Local Plan 2011-2031 (adopted 20 July 2015)**

Policy ESD 15: The Character of the Built and Historic Environment  
Policy ESD 17: Green Infrastructure

#### **Saved policies contained in the Cherwell Local Plan 1996**

C23: Retention of features contributing to character or appearance of a conservation area

#### **Cherwell Local Plan 2011-2031 – Partial Review (Adopted September 2020)**

Policy PR8 – Land east of the A44

#### **Reference should also be made to:**

The Setting of Heritage Assets Historic Environment Good Practice Advice in Planning Note 3 (Second Edition), Historic England 2017

## 6.8 Utilities and Infrastructure

In addition to the movement and blue/green infrastructure requirements set out in earlier sections, design principles for utilities and infrastructure are as follows:

### **Development principles:**

- A coordinated approach to utilities planning should ensure that utilities are provided from the outset and integrated into utilities corridors. The street layout is to be organised to minimise utilities diversions wherever possible.
- The existing power lines are to be appropriately reflected in the site layout or rerouted in agreement with utilities providers. Particular attention will be required in relation to power lines in proximity of the proposed schools where there is a requirement for them not to cross school sites and for no high voltage overhead cables to be located within 200m of any school site.
- The site is crossed by Thames Water utility lines. Thames Water must be consulted on the width of corridor they require to be reserved for future access and on the future use of the reserved corridor.
- Potential noise pollution arising from the A44 and railway line should be mitigated by following the principles of good acoustic design. For example, it is assumed that houses at the site boundaries should face onto the source of the noise to shield gardens and provide mitigation to the rest of the development site.

General requirements for infrastructure provision are set out in the LPPR Infrastructure Schedule (Appendix 4).

### **Cherwell Local Plan 2011-2031 (adopted 20 July 2015)**

Policy BSC 9: Public Services and Utilities  
Policy INF 1: Infrastructure

### **Cherwell Local Plan 2011-2031 – Partial Review (Adopted September 2020)**

Policy PR8 – Land east of the A44  
Policy PR11 – Infrastructure Delivery

### **Cherwell Residential Design Guide SPD (adopted 16 July 2018)**

Chapter 5: Streets and Spaces

## 7.0 Delivery and Monitoring

### 7.1 Information to Accompany Planning Applications

In accordance with Policy PR8 a single comprehensive, outline scheme shall be approved for the entire site.

The check list below provides an indication of documents required at application stage. It is recommended that pre-application discussions are undertaken with Cherwell District Council prior to the submission of planning applications to agree the scope of the documentation to be provided.

- Delivery and Phasing Plan
- Planning Statement
- Housing Mix and Affordable Housing Provision
- Design and Access Statement
- Topographical Surveys
- Masterplan and Parameter Plans
- Landscape Visual Impact Assessment
- Arboricultural Impact Assessment
- Transport Assessment and Framework Travel Plan
- Parking Principles (where not covered in the Brief)
- Public right of way assessment and improvement statement
- Flood Risk Assessment and Drainage Assessment (foul and surface water drainage) including Water Infrastructure Capacity
- Air Quality Assessment and Low Emission Strategy
- Contamination Assessment
- Noise and Vibration Assessment
- Archaeological Surveys and Strategy
- Heritage Impact Assessment
- Heritage Assessment
- Construction Implementation
- Construction and Environmental Management Plan
- Construction Traffic Management Plan
- Ecological surveys and Biodiversity Impact Assessment (including a Habitat Suitability Index)
- Biodiversity Improvement and Management Plan
- Landscape and Ecological Management Plan
- Energy Strategy / Sustainability Principles and Sustainable Construction Strategy
- Employment, Skills and Training Plan
- Health Impact Assessment (see Oxfordshire Health Impact Assessment Toolkit, 2021: <https://futureoxfordshirepartnership.org/wp-content/uploads/2021/01/210126-Oxon-HIA-Toolkit-FINAL.pdf>)
- Community Involvement Statement
- Management Plan for the appropriate re-use and improvement of soils
- Services and Utilities
- Management and Maintenance Strategy for all Public Open Space
- S106 Draft Heads of Terms
- Youth and Play Strategy
- Movement Strategy
- Public Art Strategy

A Scoping Opinion was issued by CDC in July 2021 in relation to the requirement for Environmental Impact Assessment (CDC Ref:21/01635/SCOP). As a result, an Environmental Impact Assessment is to be prepared which takes into consideration the matters which have been 'scoped in'.

Any detailed planning applications or reserved matters applications should also include:

- Environmental Statement compliance statement
- Design Code/Development Brief compliance statement
- Materials Schedule
- Boundary Treatment Plan
- Landscape Strategy including Soft and Hard Landscape Plans
- Parking Strategy
- Services and Utilities Plan
- Waste and Recycling Plan including bin storage and bin collection points
- Heritage Statement (where appropriate)

The use of conditions to secure this additional detail will not generally be supported by the local planning authority.

## 7.2 Securing comprehensive development

It is essential that the site is developed in a comprehensive manner to deliver the site-specific requirements in Policy PR8 and support the wider aims of the LPPR spatial strategy.

Where land, services or infrastructure within the site is designed to serve wider Cherwell Local Plan Partial Review developments, planning applications will demonstrate how this can be co-ordinated and delivered effectively through site masterplanning and S106 agreements.

Any infrastructure links or open space networks that are common to more than one Cherwell Local Plan Partial Review development site will be either constructed to the site boundary or in such a way as to facilitate connection, where required, between development sites with access to residents/public provided so as to avoid a 'ransom' position being established which prejudices the effective delivery of this common infrastructure and/or its long-term community benefit.

The development brief's site-specific vision, development principles and 'parameter plans' have been prepared to ensure a comprehensive development in compliance with Plan policies.

In line with Policy PR8 a single comprehensive, outline scheme is to be approved for the entire site supported by a Delivery Plan across the multiple planning applications which the Council expects to be submitted.

The Delivery and Phasing Plan accompanying the planning application(s) is expected to demonstrate how the implementation and phasing of the development shall be secured comprehensively and how individual development parcels, including the provision of supporting infrastructure, will be delivered.

Obligations are to be secured via a planning agreement, entered into under section 106 of the Town and Country Planning Act 1990. Consistent with national planning policy and practice guidance and the Cherwell Developer Contributions SPD (February 2018), the allocation of S106 costs required to serve the development is to be agreed with the applicant to secure appropriate financial contributions and/or in-kind works under a direct delivery obligation. Subject to statutory tests, these shall provide for "on site" and/or "offsite" facilities and infrastructure as required.

In preparing a draft Head of Terms, it is recommended that applicants should have regard to matters including the LPPR Infrastructure schedule Where facilities and infrastructure are required to be provided on land outside the site, these are to be secured by way of proportionate planning obligations and/or through the pooling of contributions as appropriate, in accordance with the Community Infrastructure Levy Regulations 2010, as amended.

It is recommended that pre-application discussions are undertaken with Cherwell District Council ahead of submitting the draft Head of Terms for developer contributions. In preparing a draft Head of Terms, it is recommended that proposals have regard to matters including the LPPR Infrastructure schedule and should consider in discussions with infrastructure providers whether infrastructure issues will require the phasing of development to ensure that necessary services, facilities or apparatus are provided in advance if needed

Further guidance is contained in the Cherwell Developer Contributions SPD (February 2018).

### 7.3 Monitoring

Monitoring will be undertaken in accordance with Policy PR13 –Monitoring and Securing Delivery. The delivery of LPPR proposals will be monitored through the Council's Annual Monitoring Report process.

**Cherwell Local Plan 2011-2031 (adopted 20 July 2015)**  
Policy INF 1: Infrastructure

**Cherwell Local Plan 2011-2031 – Partial Review (adopted September 2020)**  
Policy PR8 – Land east of the A44  
Policy PR11 – Infrastructure Delivery  
PR12a-Delivering Sites and Maintaining Housing Supply  
Policy PR13 – Monitoring and Securing Delivery  
Appendix 3 – Housing Trajectory

**Cherwell Developer Contributions SPD (adopted February 2018)**



# Appendix A: Relevant Development Plan Policies & Supplementary Planning Documents

## **Cherwell Local Plan 2011-2031 (Part 1) Partial Review, the “LPPR”:**

- PR1 – Achieving Sustainable Development for Oxford’s Needs
- PR2 – Housing Mix, Tenure and Size
- PR3 – The Oxford Green Belt
- PR4a – Sustainable Transport
- PR4b – Kidlington Centre
- PR5 – Green Infrastructure
- Policy PR8 – Land east of the A44
- PR11 – Infrastructure Delivery
- PR12a – Delivering Sites and Maintaining Housing Supply
- PR12b – Sites Not Allocated in the Partial Review
- PR13 – Monitoring and Securing Delivery

## **Cherwell Local Plan 2011-2031 “The 2015 Plan”:**

- PSD1 – Presumption in Favour of Sustainable Development
- SLE4 - Improved Transport and Connections
- BSC2 – The Effective and Efficient Use of Land, Brownfield Land and Housing Density
- BSC3 – Affordable Housing
- BSC4 – Housing Mix Policy
- BSC7 – Meeting Education Needs
- BSC8 – Securing Health and Well-Being
- BSC9 – Public Services and Utilities
- BSC10 – Open Space, Outdoor Sport and Recreation Provision
- BSC11 – Local Standards of Provision – Outdoor Recreation
- BSC12 – Indoor Sport, Recreation and Community Facilities
- ESD1 – Mitigating and Adapting to Climate Change
- ESD2 – Energy Hierarchy and Allowable Solutions
- ESD3 – Sustainable Construction
- ESD4 – Decentralised Energy Systems
- ESD5 – Renewable Energy
- ESD6 – Sustainable Flood Risk Management
- ESD7 – Sustainable Drainage Systems
- ESD8 – Water Resources
- ESD9 – Protection of Oxford Meadows SAC
- ESD10 – Protection and Enhancement of Biodiversity and the Natural Environment
- ESD11 – Conservation Target Areas
- ESD13 – Local Landscape Protection and Enhancement
- ESD14 – Oxford Green Belt
- ESD15 – The Character of the Built and Historic Environment
- ESD17 – Green Infrastructure
- INF1 - Infrastructure

**Adopted Cherwell Local Plan 1996**

- GB2 - Change of use of land within the Green Belt
- TR1 - Transportation Funding
- TR11 – Oxford Canal
- TR22 - Roads
- C5 – Ecological Value of Features
- C14 – Trees and Landscaping
- C18 – Development proposals affecting a listed building
- C21 – Re-Use of Listed Buildings
- C23 – Conservation Areas
- C25 – Scheduled Ancient Monument
- C28 – Design Quality
- C29 – Design and The Oxford Canal
- C30 – Design Control
- C31 - Amenity
- C32 – Disabled Access
- ENV1 – Environmental Pollution
- ENV10 – Hazardous Installations
- ENV12 – Contaminated Land

**Adopted SPD**

- Cherwell Residential Design Guide (July 2018)
- Developer Contributions (February 2018)
- Kidlington Masterplan (December 2016)

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